

ONE VANDERBILT



BONUSABLE IMPROVEMENTS

ESTIMATING PROCESS & METHODOLOGY

- Extensive 17-month process involving MTA, New York City Transit, East Side Access, LIRR, Metro North and their respective outside engineering consultants for all transit related improvement projects
- For the Lexington Avenue Subway Station improvements, the estimates were prepared based on schematic level plans previously prepared by Parsons Brinckerhoff through New York City Transit
- Conducted walkthroughs of existing conditions in and around the Lexington Avenue Subway Station to gain an understanding of the existing conditions and complexities involved
- Multiple working sessions were held with MTA – NYCTA including their outside engineering team over a 6-month period to confirm:
 - Scopes of work involved with each improvement
 - Special conditions and specifications for proposed equipment and finishes
 - Boundaries of proposed work
 - Work hours and restrictions due to active train schedules
 - Schedule and phasing of each project (spanning a period of 5-6 years total)
 - Material quantities
 - Constructability
- The conclusion of our work resulted in a validation of final estimates by the MTA and their outside engineering consultants
- Estimate includes quantities where reasonably measured, otherwise experienced judgments were made from the information available to arrive at reasonable estimates
- On-site improvement costs exclude the opportunity cost of using the space for other purposes and exclude the value of the underlying real estate

ESTIMATING PROCESS

Key Team Members

Tishman Construction Corp.

Tishman's construction services team provides clients with an extensive range of pre-construction and construction-related services and solutions for projects of varying scope, budget, schedule and complexity. Infrastructure and transportation projects are often constructed within complex urban environments which require work to be phased around existing operations. Tishman has successfully managed the complexities of constructing and renovating major rail, bus, and ferry terminals, storage facilities, airports, parking garages, as well as performed major infrastructure and security system upgrades and maintenance work within active mass transit systems.

Joe Ross

Executive Vice President; 41 years

John Glynn

Executive Vice President - Preconstruction; 27 years

John Bonse

Senior Estimator; 9 Years

Notable Transit Projects:

Moynihan Station, World Trade Center Transportation Hub,
Hoboken Terminal Renovation, WTC Vehicle Security Center

Stantec

Stantec is an international engineering firm of more than 14,000 employees working in over 230 locations. They collaborate across disciplines and industries to bring buildings, energy and resource, and infrastructure projects to life. Their work includes professional consulting in planning, engineering, architecture, interior design, landscape architecture, surveying, environmental sciences, and project management.

James Ariola

Senior Principal, Transportation; 31 years

Stuart Lerner

Vice President, Transit Rail; 32 years

Stephane Lefebvre

Senior Engineer; 11 years

Notable Transit Projects:

Times Square Station, Columbus Circle,
Fulton Street Roosevelt Avenue & 74th Street

COST ESTIMATE

Summary of Onsite and Offsite Improvements

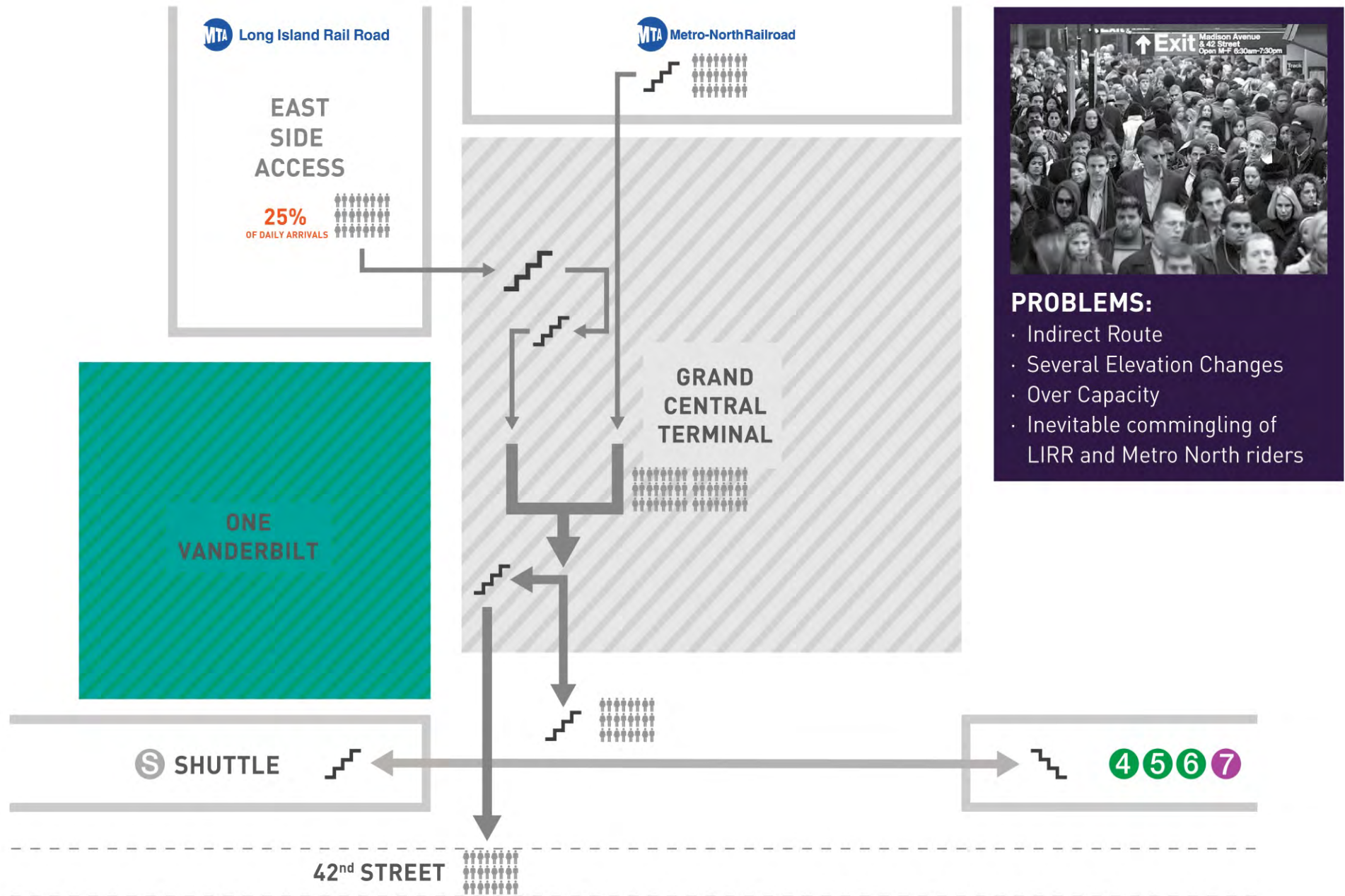
Project		Cost
1	East Side Access Connection	\$16,578,500
2	Shuttle Improvement	\$15,723,500
3	Grand Central Intermodal Connection	\$18,098,500
4	Public Transit Hall	\$9,800,200
5	Vanderbilt Public Plaza	\$10,786,000
4/5/6/7 Lexington Subway Station Improvements:		
6	Mezzanine Level Circulation Space	\$47,200,250
7	Hyatt North Stairs	\$37,384,200
8	Modifications of Existing Stairs to Platform Level	\$27,434,500
9	Replacement of Street Entrance at Lexington	\$14,523,500
10	New Street Stairs and Passageway at Mobil	\$7,347,500
11	New Stair to Mezzanine Below 125 Park Avenue	<u>\$5,311,500</u>
4/5/6/7 Subtotal:		\$139,201,500
Total Estimate of Improvements:		\$210,188,000

EAST SIDE ACCESS CONNECTION

PUBLIC BENEFITS

Transit to Transit & Points Southwest: Current Condition (2023 – Projected ESA Completion)

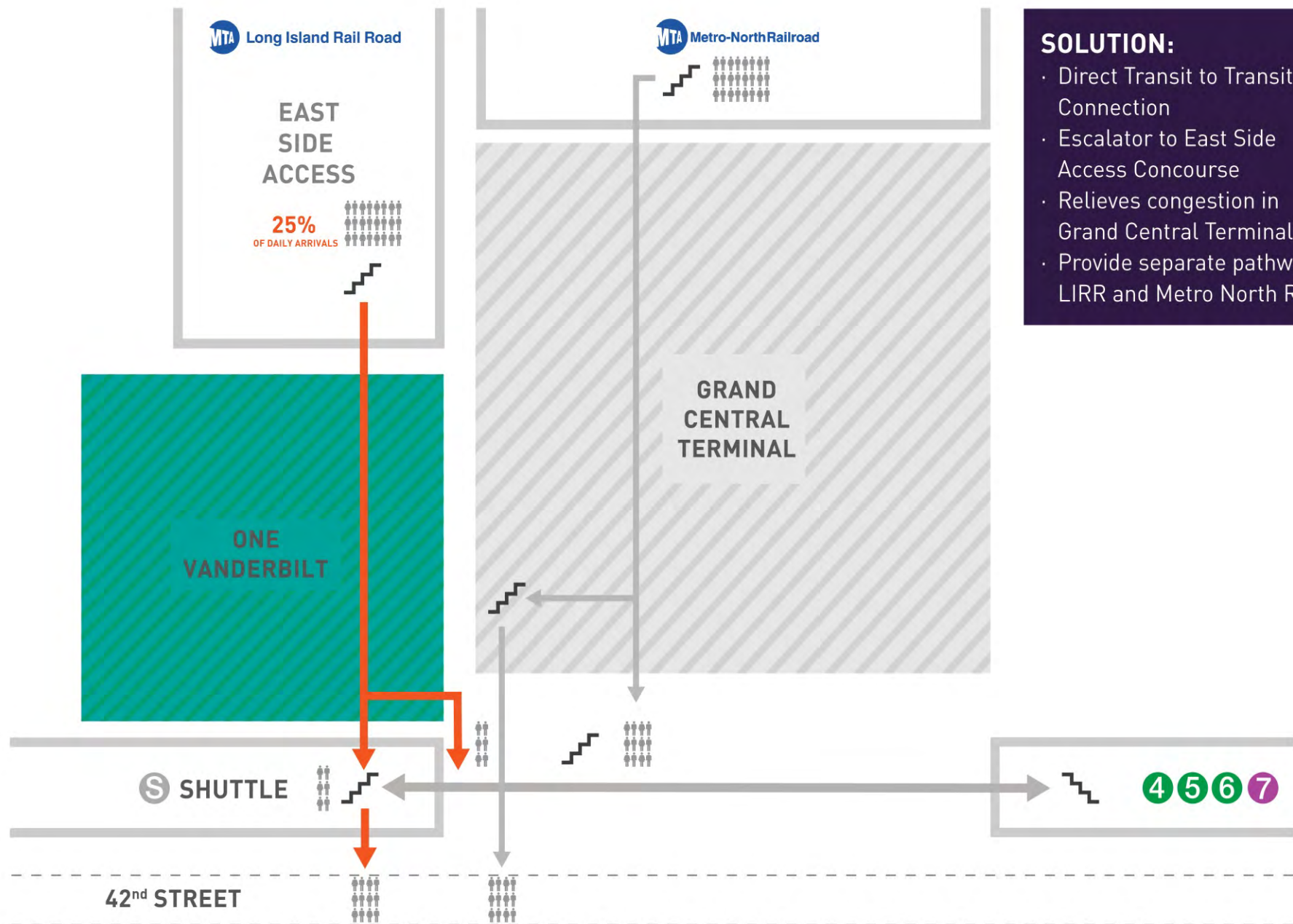
1



PUBLIC BENEFITS

Transit to Transit & Points Southwest: Solution

1



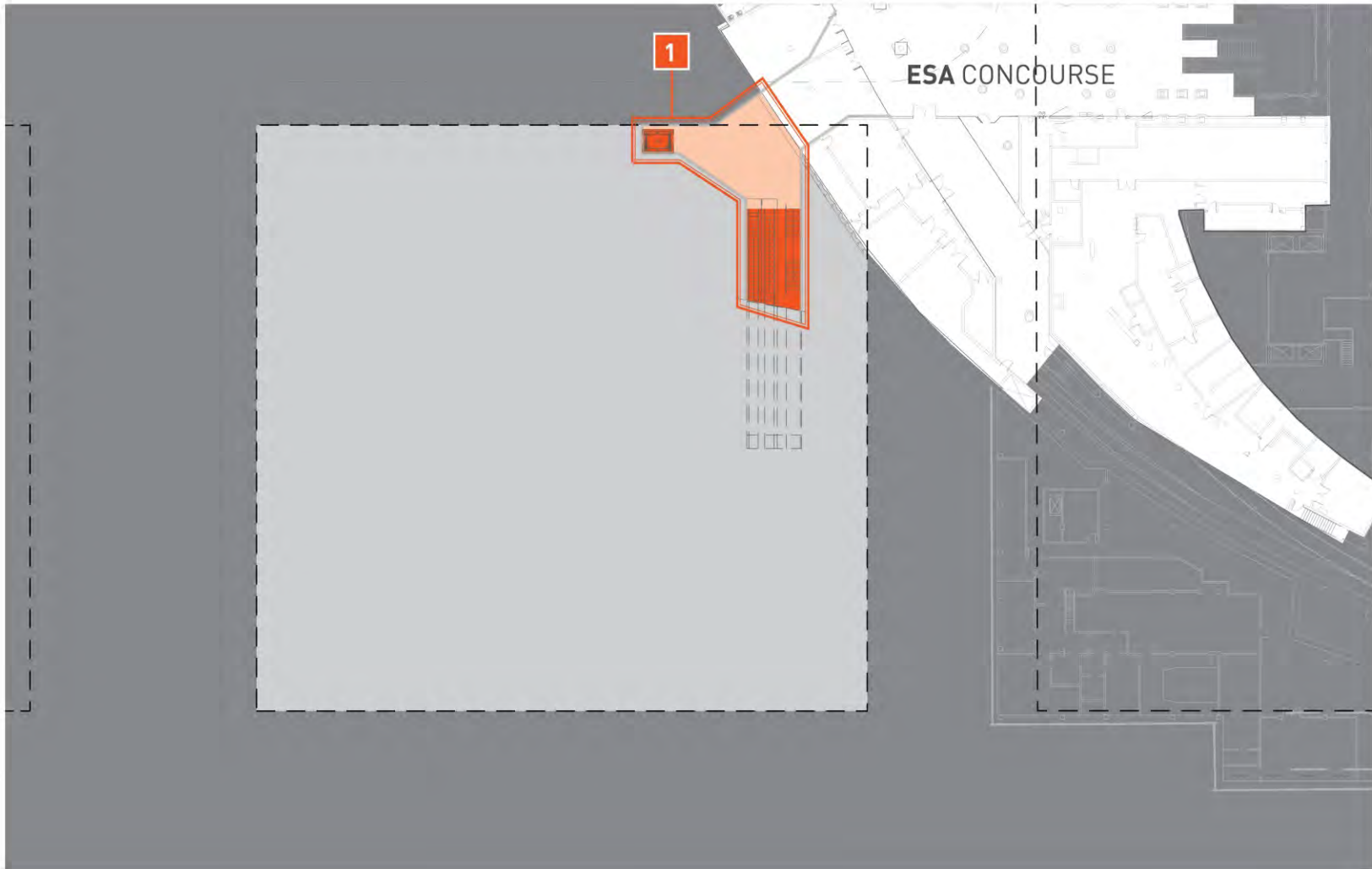
SOLUTION:

- Direct Transit to Transit Connection
- Escalator to East Side Access Concourse
- Relieves congestion in Grand Central Terminal
- Provide separate pathways for LIRR and Metro North Riders

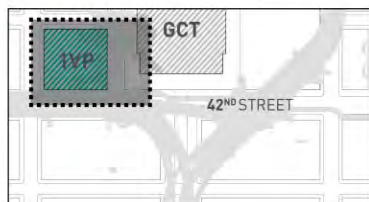
BONUSABLE IMPROVEMENTS

Onsite: East Side Access Connection

1



Key Map



Legend

1 NEW CONNECTION TO EAST SIDE ACCESS CONCOURSE

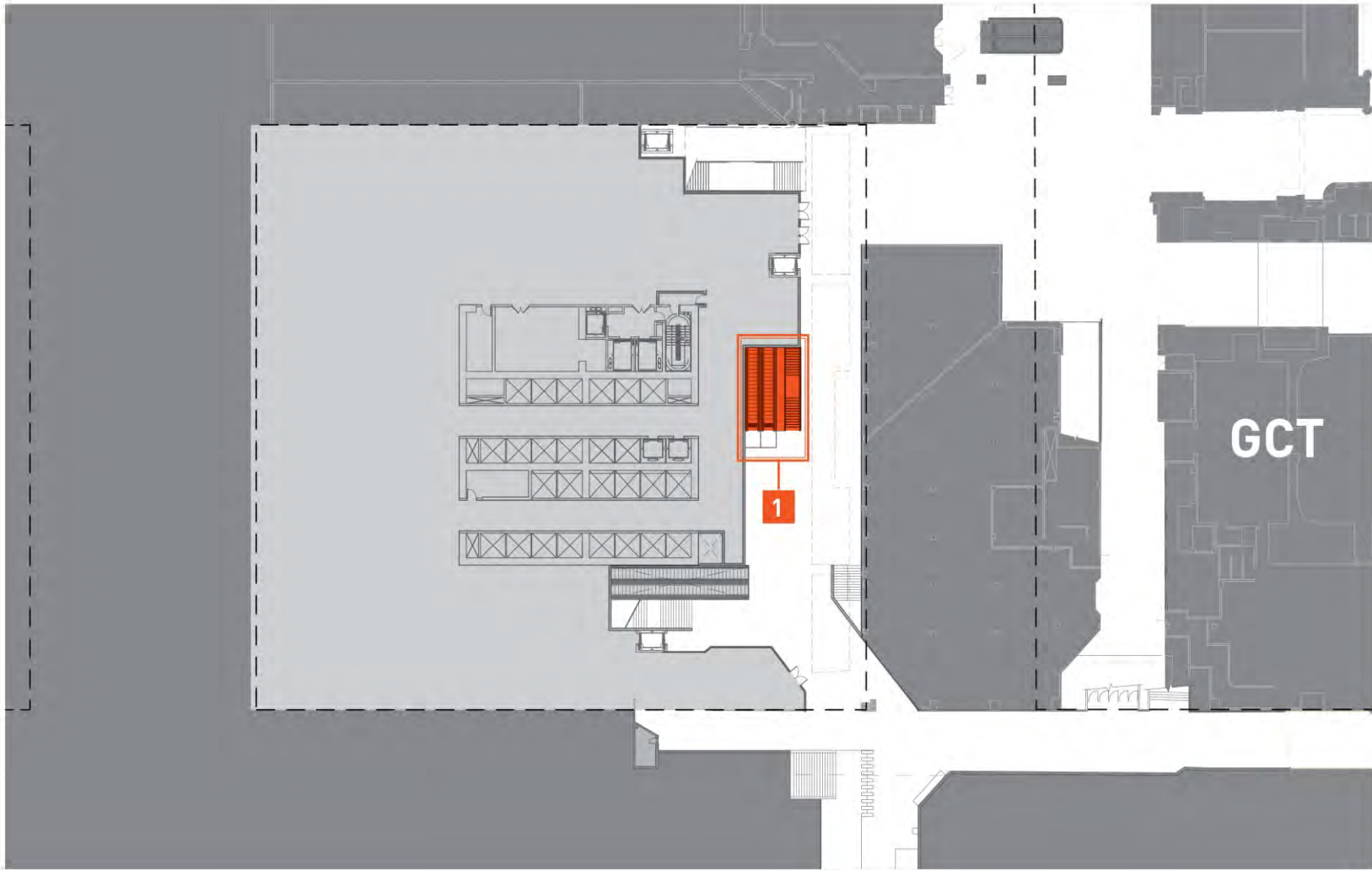
Floor Plan

B3 BELOW GRADE OF ONE VANDERBILT CONNECTION TO EAST SIDE ACCESS CONCOURSE

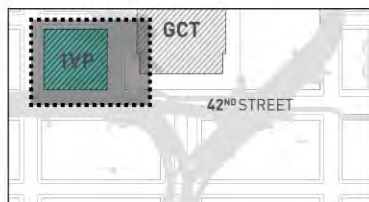
BONUSABLE IMPROVEMENTS

Onsite: East Side Access Connection

1



Key Map



Legend

1 NEW CONNECTION TO EAST SIDE ACCESS CONCOURSE

Floor Plan

B1 BELOW GRADE OF ONE VANDERBILT CONNECTION TO GCT MAIN CONCOURSE

BONUSABLE IMPROVEMENTS

One Site: East Side Access Connection

1



PROPOSED MTA CONCOURSE, LOOKING AT ESA ESCALATOR AND STAIR TO STREET



COST ESTIMATE

Onsite: East Side Access Connection

1

Category	Cost
Construction Cost	\$12,899,550
<ul style="list-style-type: none">– Direct Trade Work– Contractors General Conditions– Contractor Bonds and Insurance– Contractor Overhead and Fee– Anticipated Costs to Finalize Design Details– Construction Phasing and Logistics– Temporary Protection– Safety	
Soft Cost (22%)	\$2,889,500
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections• Track Flagmen	
Project Contingency (5%)	\$789,450
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk	
Project Total Cost Estimate:	\$16,578,500

Key Components of Work

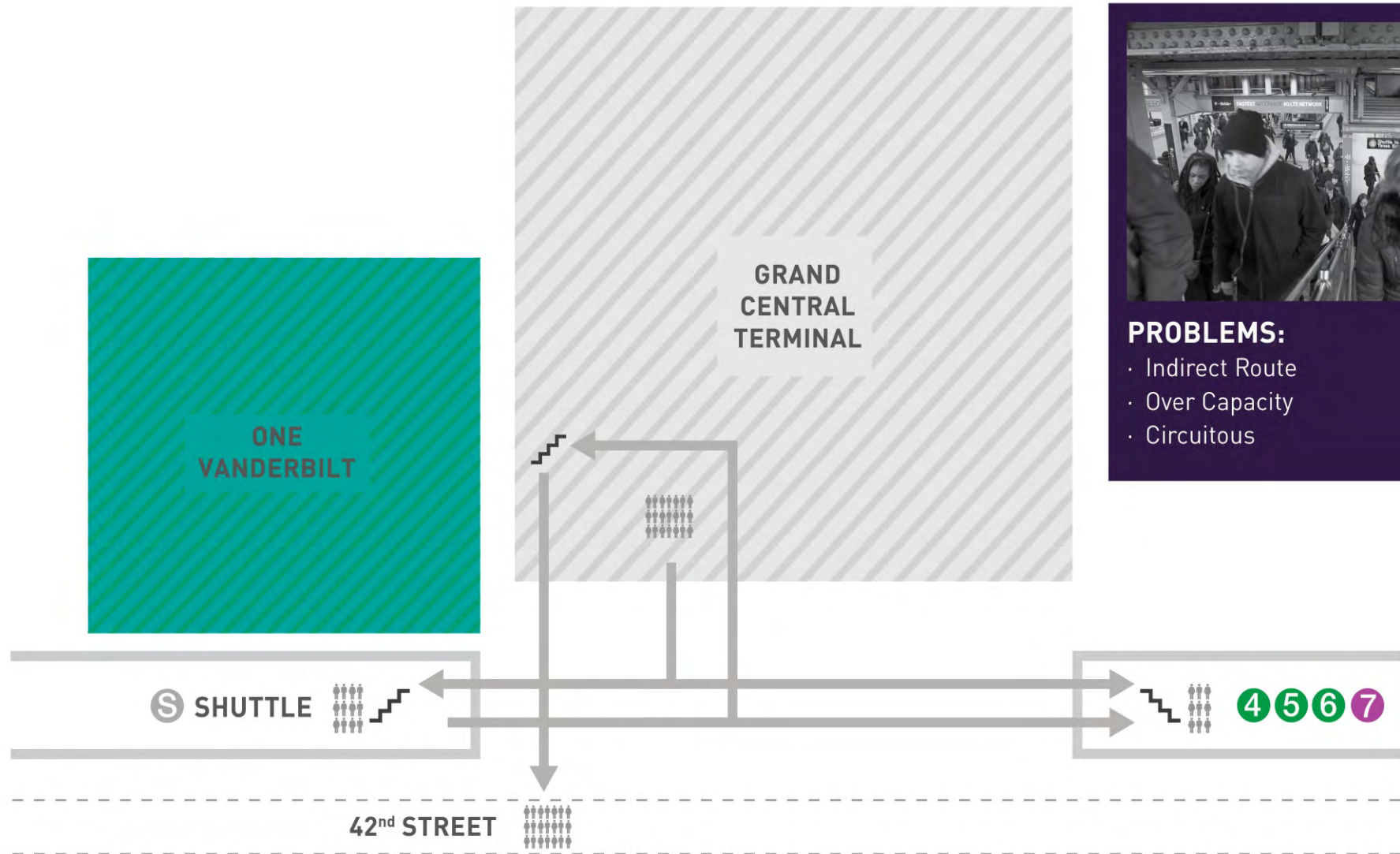
- Excavation of approximately 2,300 cubic yards of rock to a depth of 50' below street level and immediately adjacent to an active train-shed at Grand Central Terminal
- Relocation of an existing 4'-6" x 3'-0" active combined sewer line that currently crosses through the limits of excavation
- Relocation of retail stores and circulation spaces temporarily impacted by the construction activities
- Installation of (2) 40" wide x 85' long transit grade escalators and new stairs
- Installation of a transit grade elevator connecting stops with a distance of 36'
- 2,700 square feet of new pedestrian circulation space finished with architectural materials similar in quality to those in Grand Central Terminal and the East Side Access Main Concourse
- New mechanical, electrical and sprinkler systems
- New emergency communication systems
- New lighting and wayfinding

SHUTTLE IMPROVEMENTS

PUBLIC BENEFITS

Subway to Street: Current Condition

2



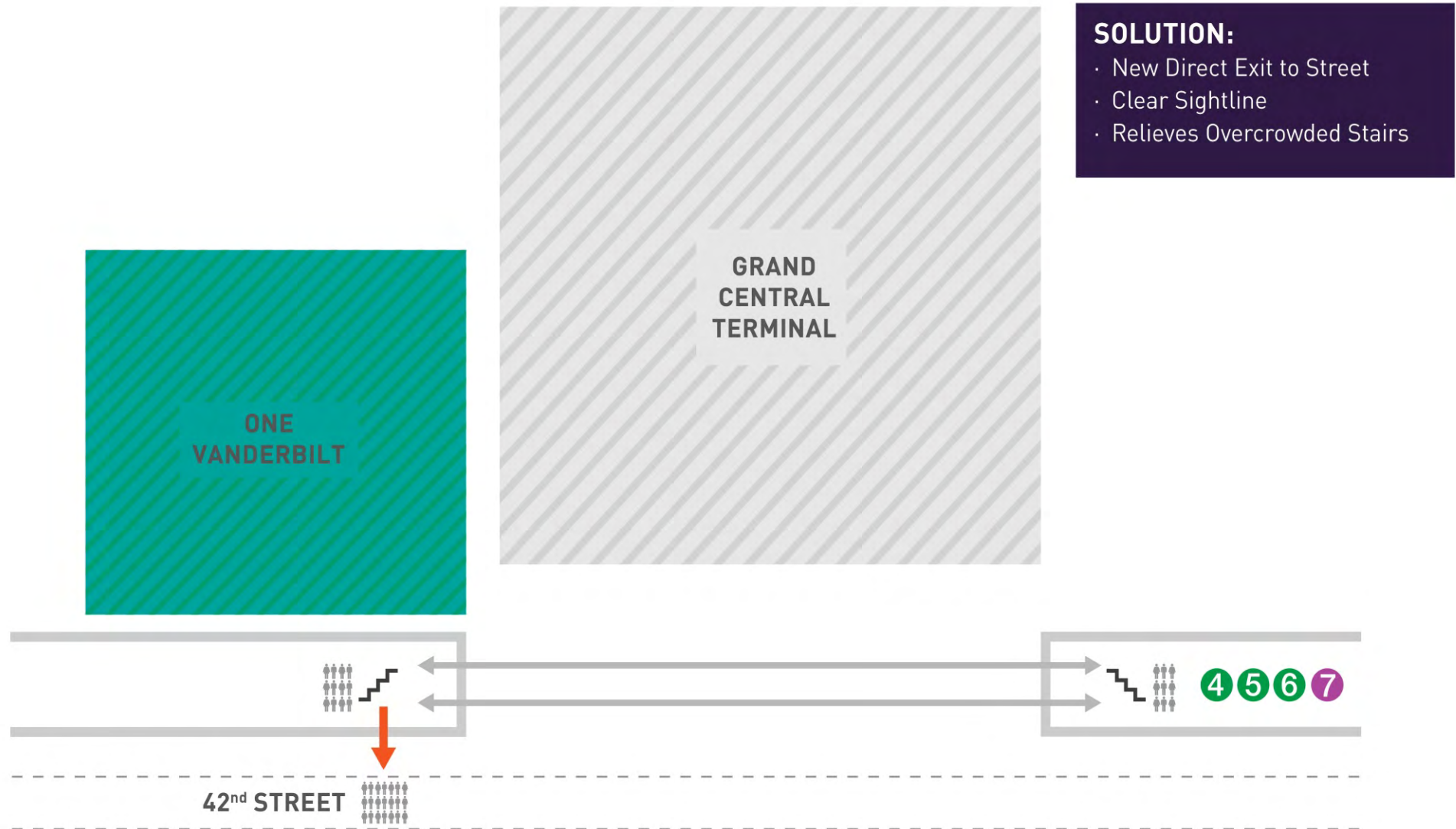
PROBLEMS:

- Indirect Route
- Over Capacity
- Circuitous

PUBLIC BENEFITS

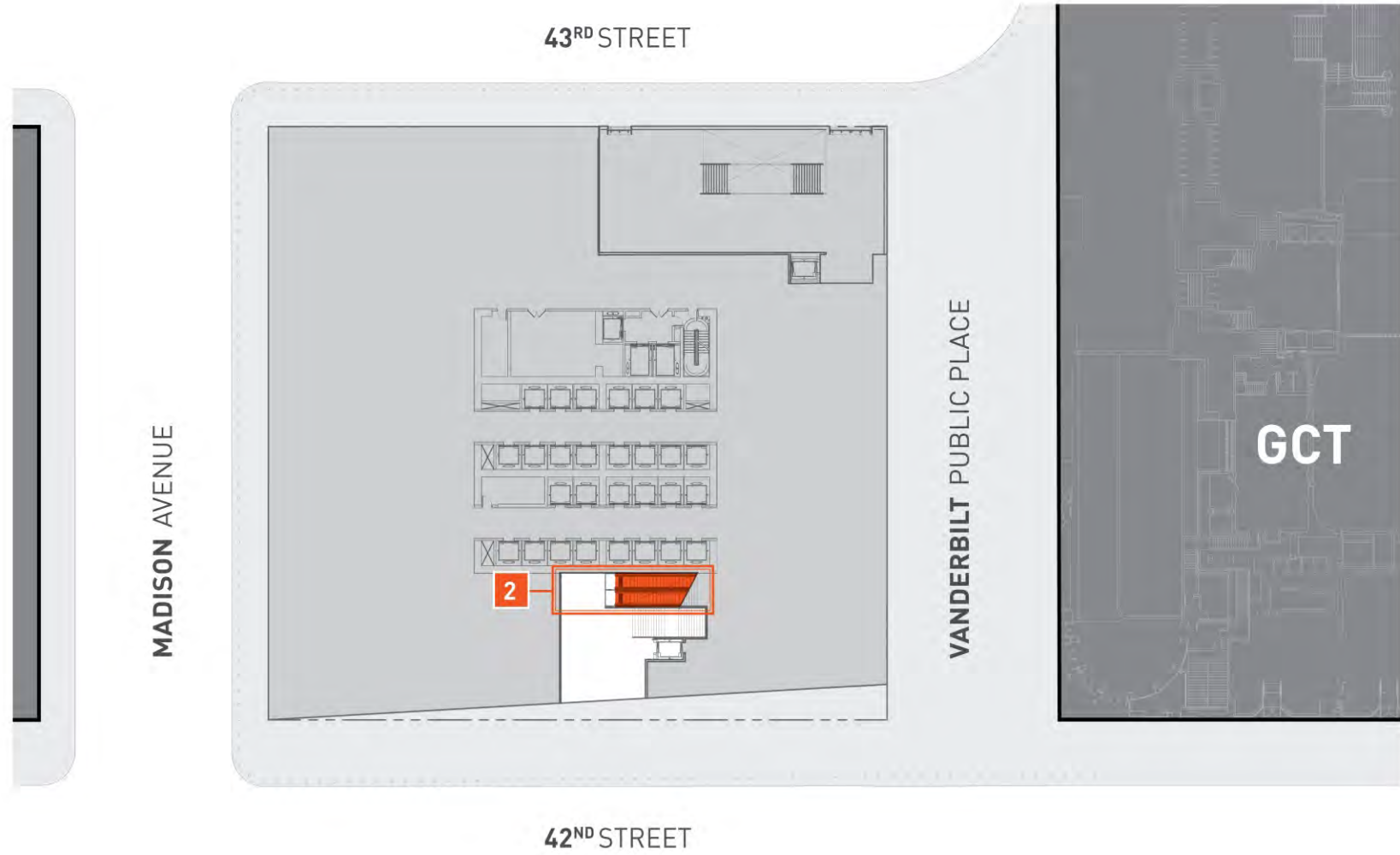
Subway to Street: Solution

2

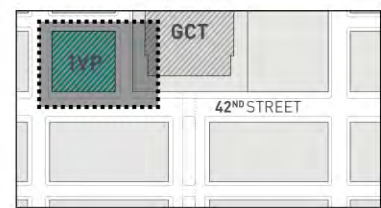


BONUSABLE IMPROVEMENTS

Onsite: Shuttle Improvement



Key Map



Legend

- 2 NEW SHUTTLE CONNECTION

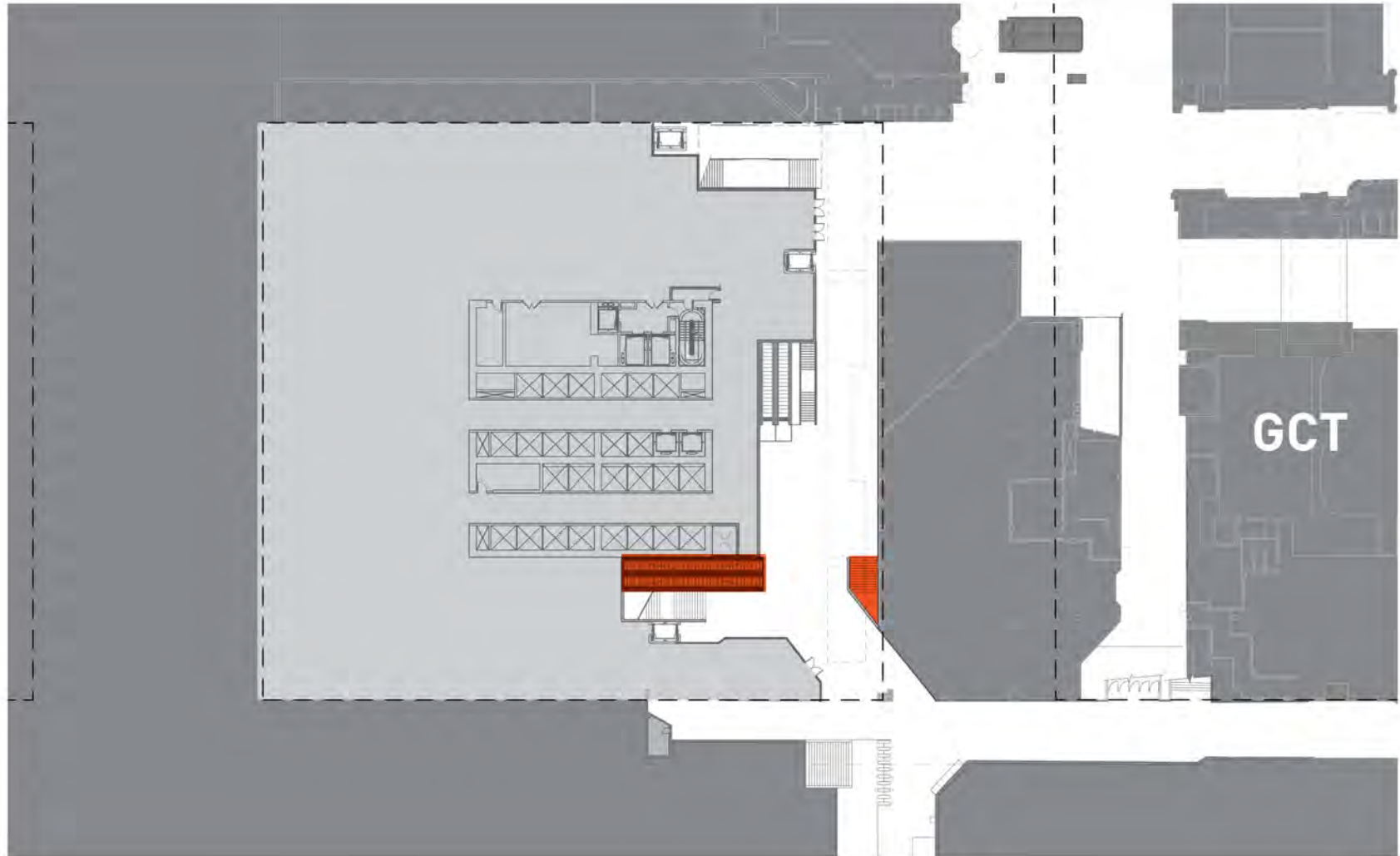
Floor Plan

- 1 GROUND FLOOR OF ONE VANDERBILT CONNECTION TO STREET

BONUSABLE IMPROVEMENTS

Onsite: Shuttle Improvement

2



Key Map



Legend

2 NEW SHUTTLE CONNECTION

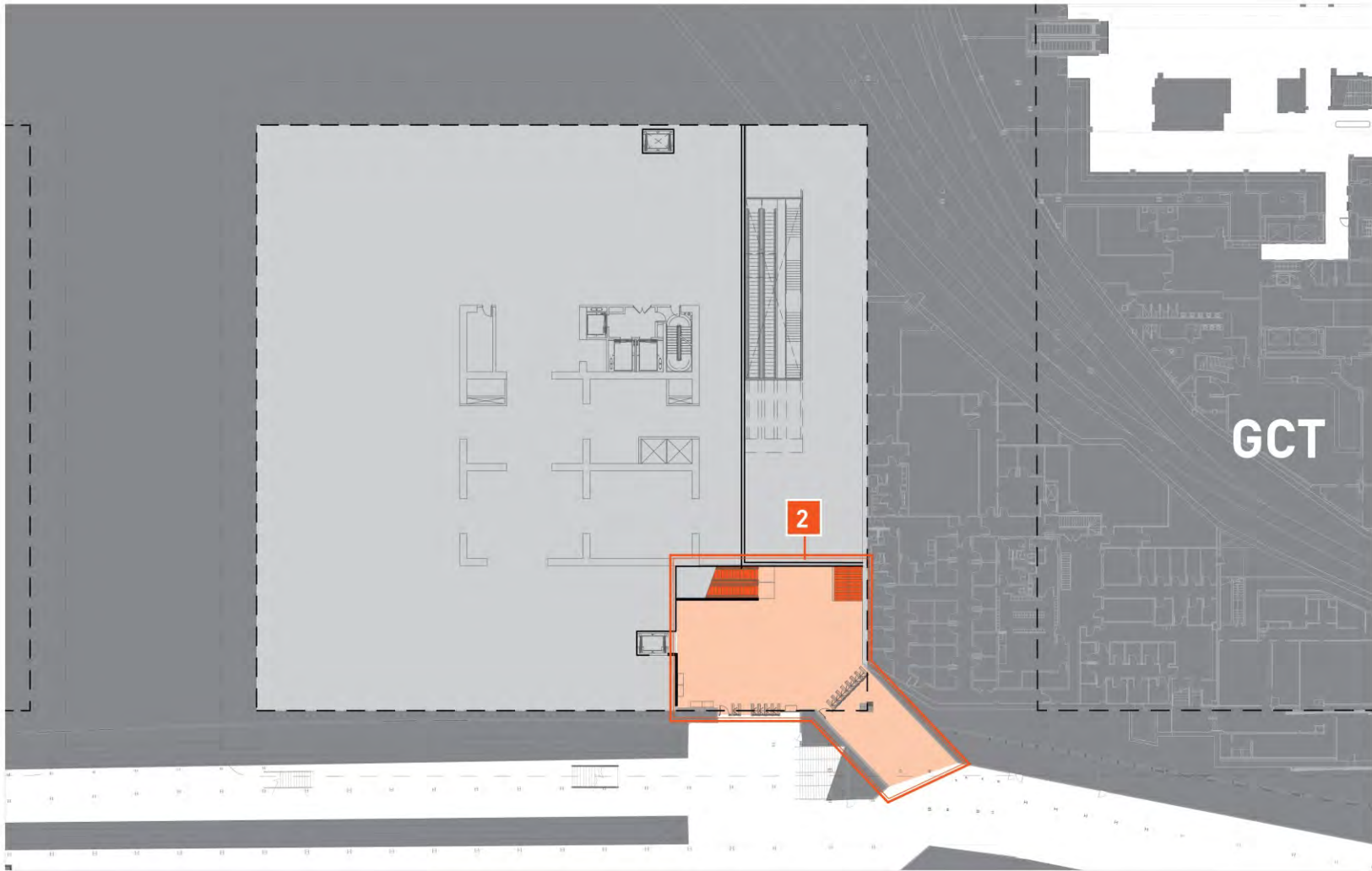
Floor Plan

B1 BELOW GRADE OF ONE VANDERBILT CONNECTION TO GCT MAIN CONCOURSE

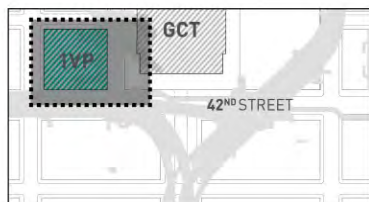
BONUSABLE IMPROVEMENTS

Onsite: Shuttle Improvement

2



Key Map



Legend

2 NEW SHUTTLE CONNECTION

Floor Plan

B2 BELOW GRADE OF ONE VANDERBILT CONNECTION TO SHUTTLE PLATFORM

COST ESTIMATE

Onsite: Shuttle Connection

2

Category	Cost
Construction Cost	\$12,234,000
<ul style="list-style-type: none">– Direct Trade Work– Contractors General Conditions– Contractor Bonds and Insurance– Contractor Overhead and Fee– Anticipated Costs to Finalize Design Details– Construction Phasing and Logistics– Temporary Protection– Safety	
Soft Cost (22%)	\$2,740,500
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections• Track Flagmen	
Project Contingency (5%)	\$749,000
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk	
Project Total Cost Estimate:	\$15,723,500

Key Components of Work

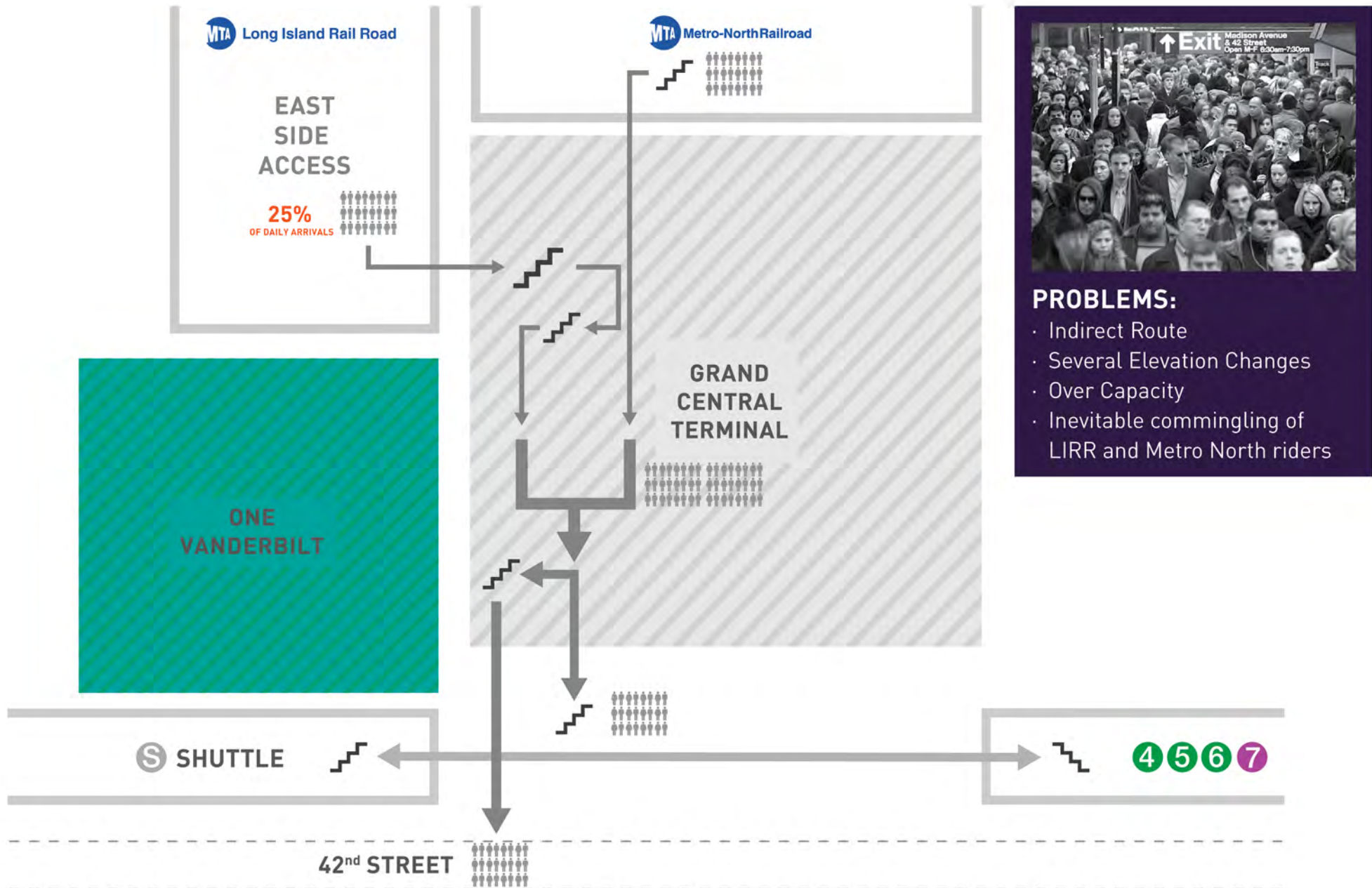
- Excavation of approximately 2,100 cubic yards of fill and rock to a depth of 26' below 42nd street and immediately adjacent to the active Shuttle Subway Station
- Extensive underpinning of existing structural components built as part of the Shuttle Subway Station
- Installation of (2) 40" wide x 75' long transit grade escalators and two new stairs
- Installation of a transit grade elevator
- 4,800 square feet of new pedestrian circulation space finished with architectural materials similar in quality to those in Grand Central Terminal and the shuttle Subway Station
- Two new connections with a total of (12) new turnstiles into the Shuttle Subway Station and Existing Passageway.
- New mechanical, electrical and sprinkler systems
- New emergency communication systems
- New lighting and wayfinding

GRAND CENTRAL INTERMODAL CONNECTOR

PUBLIC BENEFITS

Western Grand Central Terminal Access: Current Condition

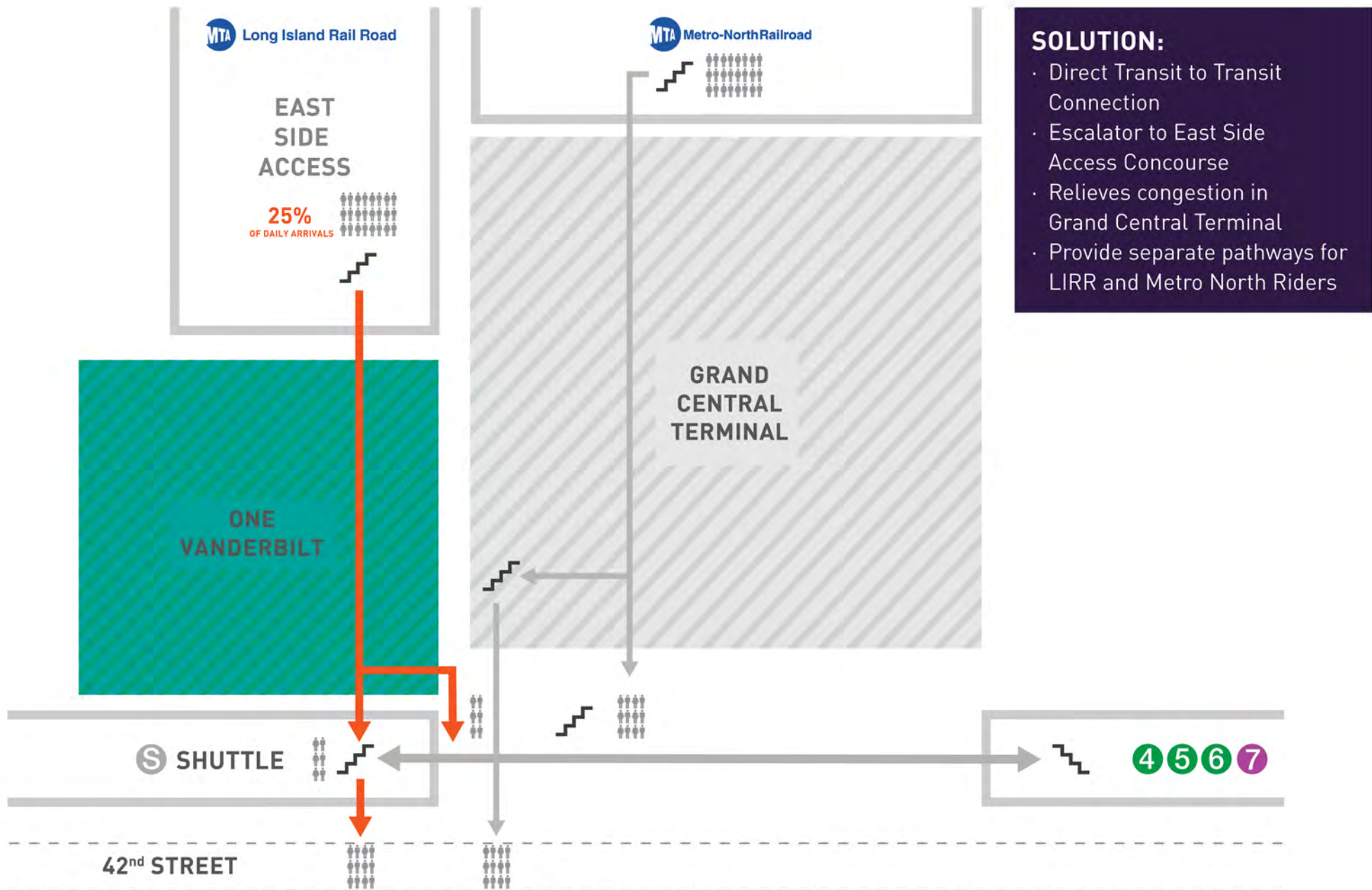
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PUBLIC BENEFITS

Western Grand Central Access: Solution

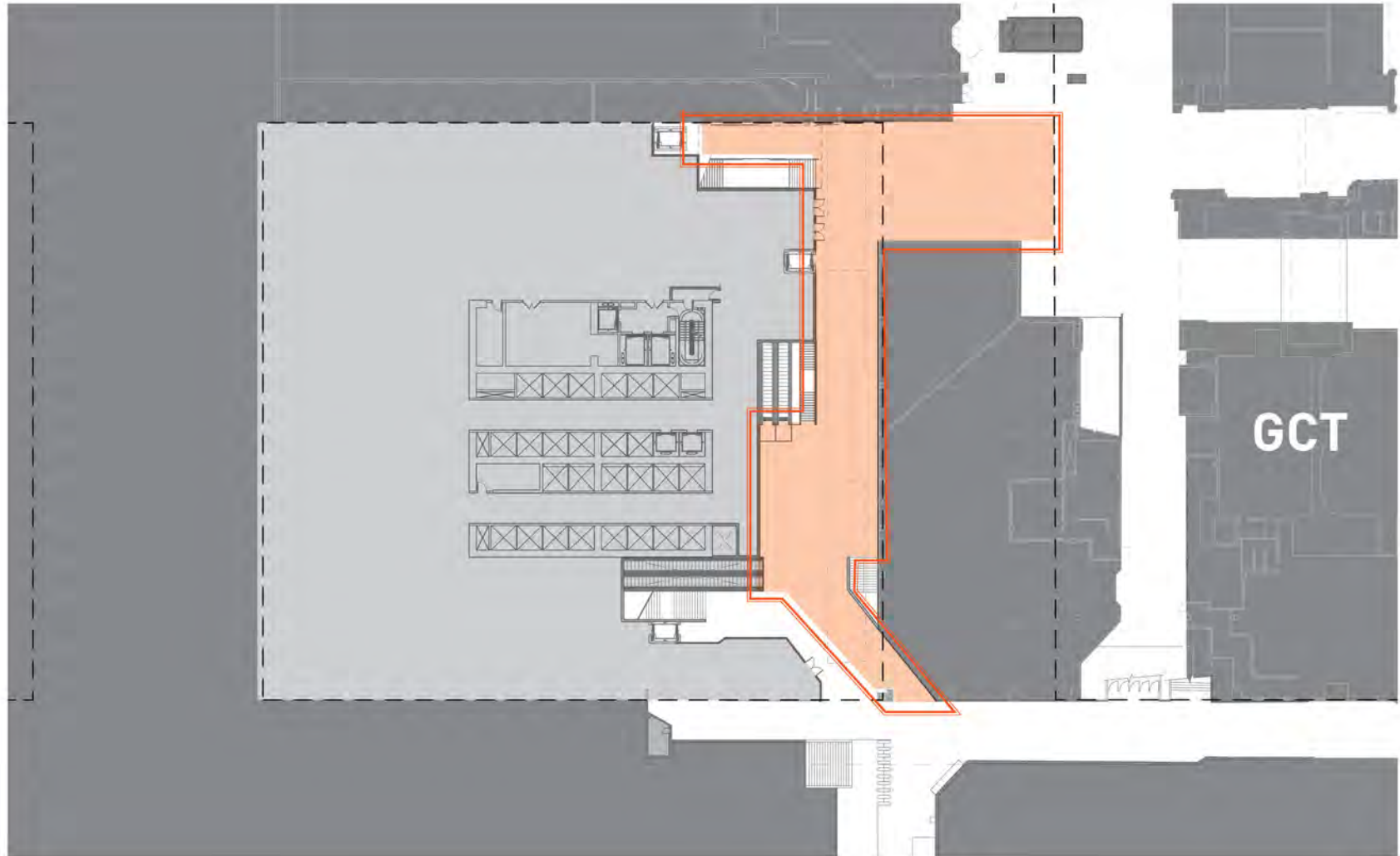
3



BONUSABLE IMPROVEMENTS

Onsite: Grand Central Intermodal Connection

3



Key Map



Legend

Floor Plan

B1 BELOW GRADE OF ONE VANDERBILT
CONNECTION TO GCT MAIN CONCOURSE

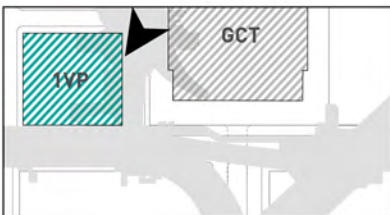
BONUSABLE IMPROVEMENTS

Onsite: Grand Central Intermodal Connection

3



PROPOSED MTA CONCOURSE, LOOKING AT TRANSIT HALL AND NEW STAIR



COST ESTIMATE

Onsite: Grand Central Intermodal Connection

3

Category	Cost	Key Components of Work
Construction Cost	\$14,082,000	<ul style="list-style-type: none">– 10,100 square feet of new intermodal pedestrian circulation space– Finishes consistent to those in Grand Central Terminal– Performing work over an active trainshed in the northeast corner of the site– 2,540 sf of new ceiling skylight glazing system for the total length to allow natural light to penetrate into the subsurface spaces
<ul style="list-style-type: none">– Direct Trade Work– Contractors General Conditions– Contractor Bonds and Insurance– Contractor Overhead and Fee– Anticipated Costs to Finalize Design Details– Construction Phasing and Logistics– Temporary Protection– Safety		
Soft Cost (14%)	\$3,154,500	
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections		
Project Contingency (5%)	\$862,000	<ul style="list-style-type: none">– New storefronts consistent in quality to those in Grand Central Terminal– New mechanical, electrical and sprinkler systems– New emergency communication systems– New lighting and wayfinding– New fire and smoke separation systems to isolate areas in the event of fire
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk		
Project Total Cost Estimate:	\$18,098,500	

PUBLIC TRANSIT HALL

PUBLIC BENEFITS

Western Grand Central Terminal Access: Current Condition

4



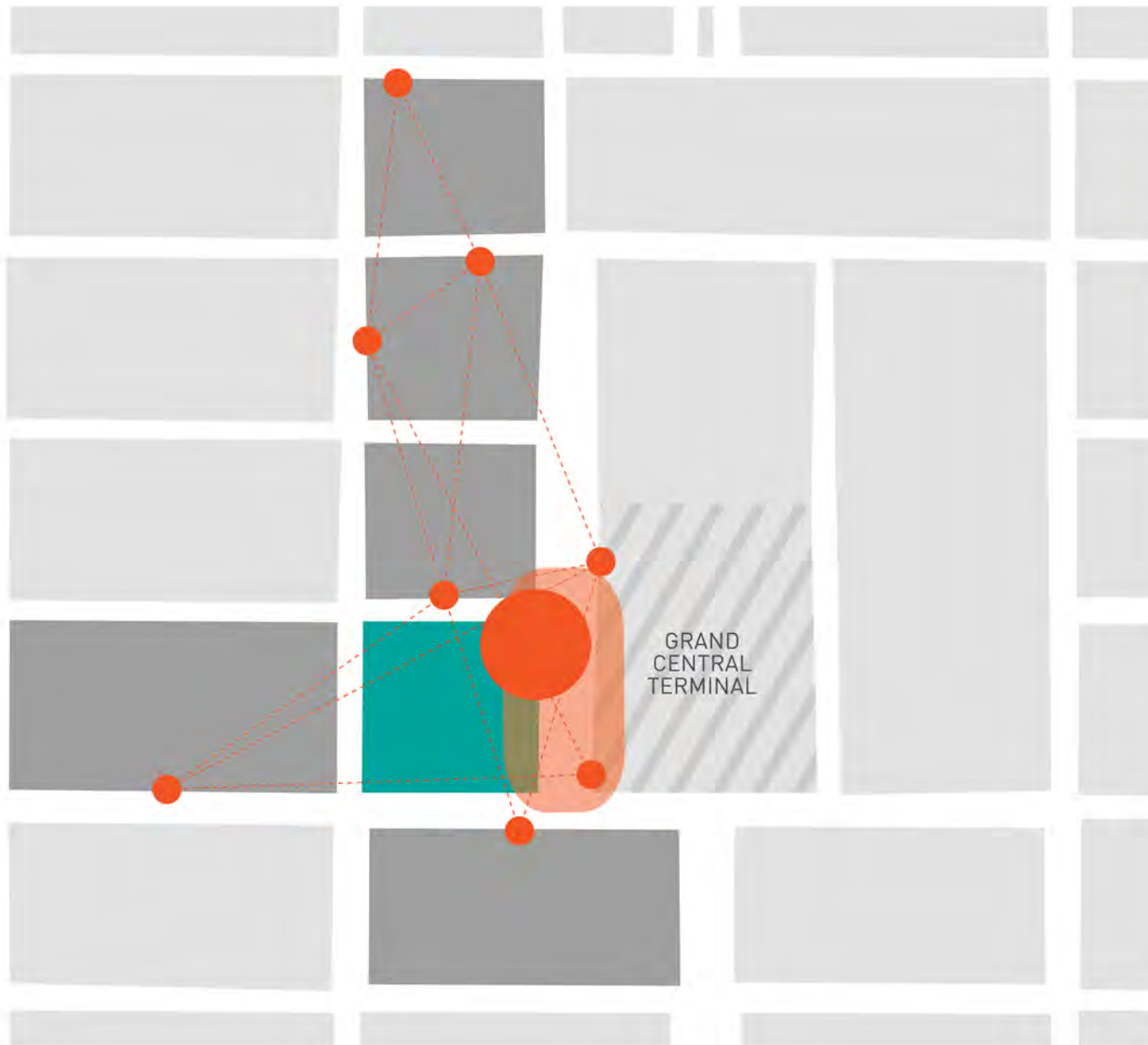
PROBLEMS:

- Decentralized Network of Exits
- Unmarked Entryways
- Underutilized Access Points
- Unattractive

PUBLIC BENEFITS

Western Grand Central Terminal Access: Solution

4



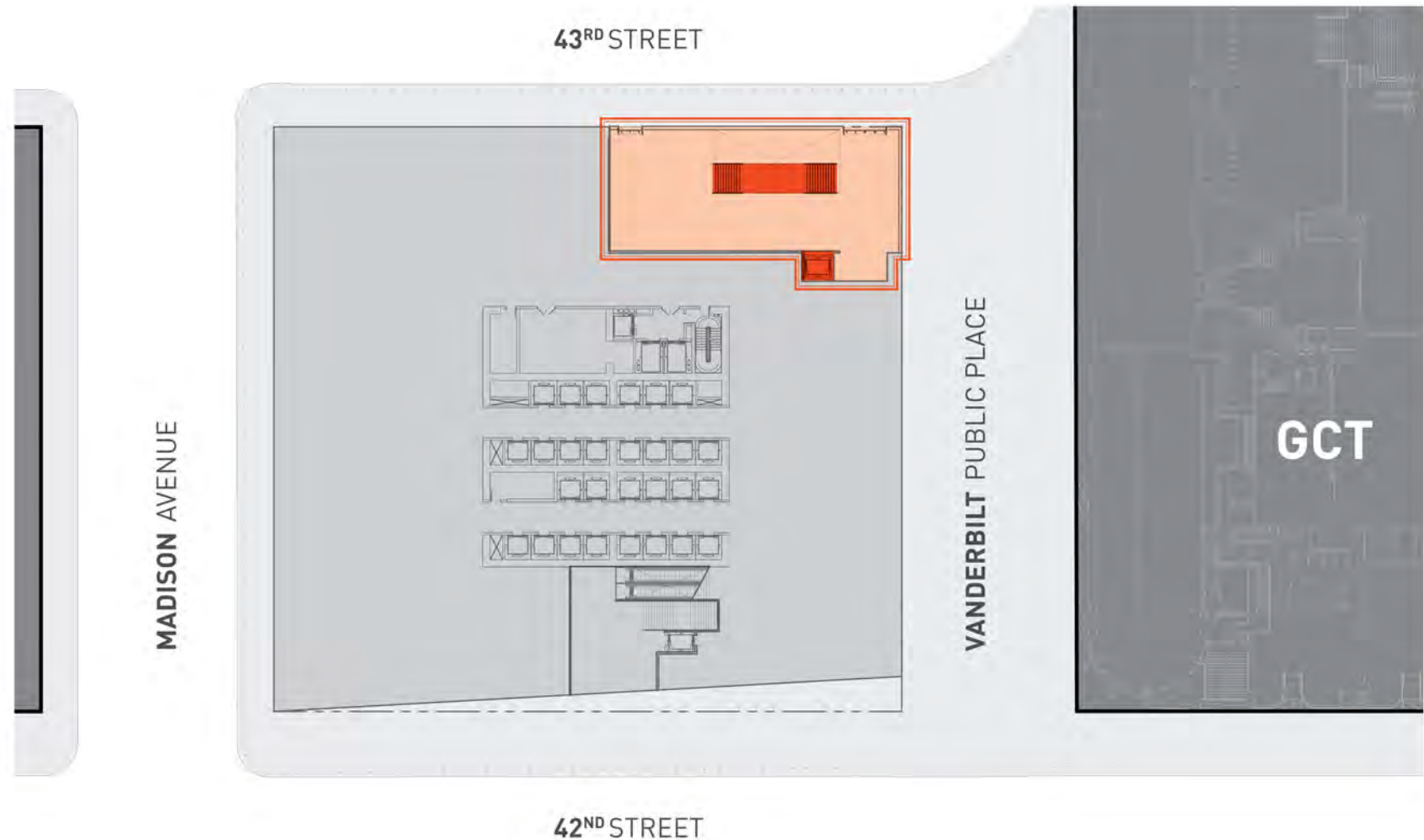
SOLUTION:

- Centralized Point of Access on the West
- Highly Visible Public Entry
- Transit Hall → Grand Central West Waiting Room
- Vanderbilt Public Place

BONUSABLE IMPROVEMENTS

Onsite: Public Transit Hall

4



Key Map



Legend

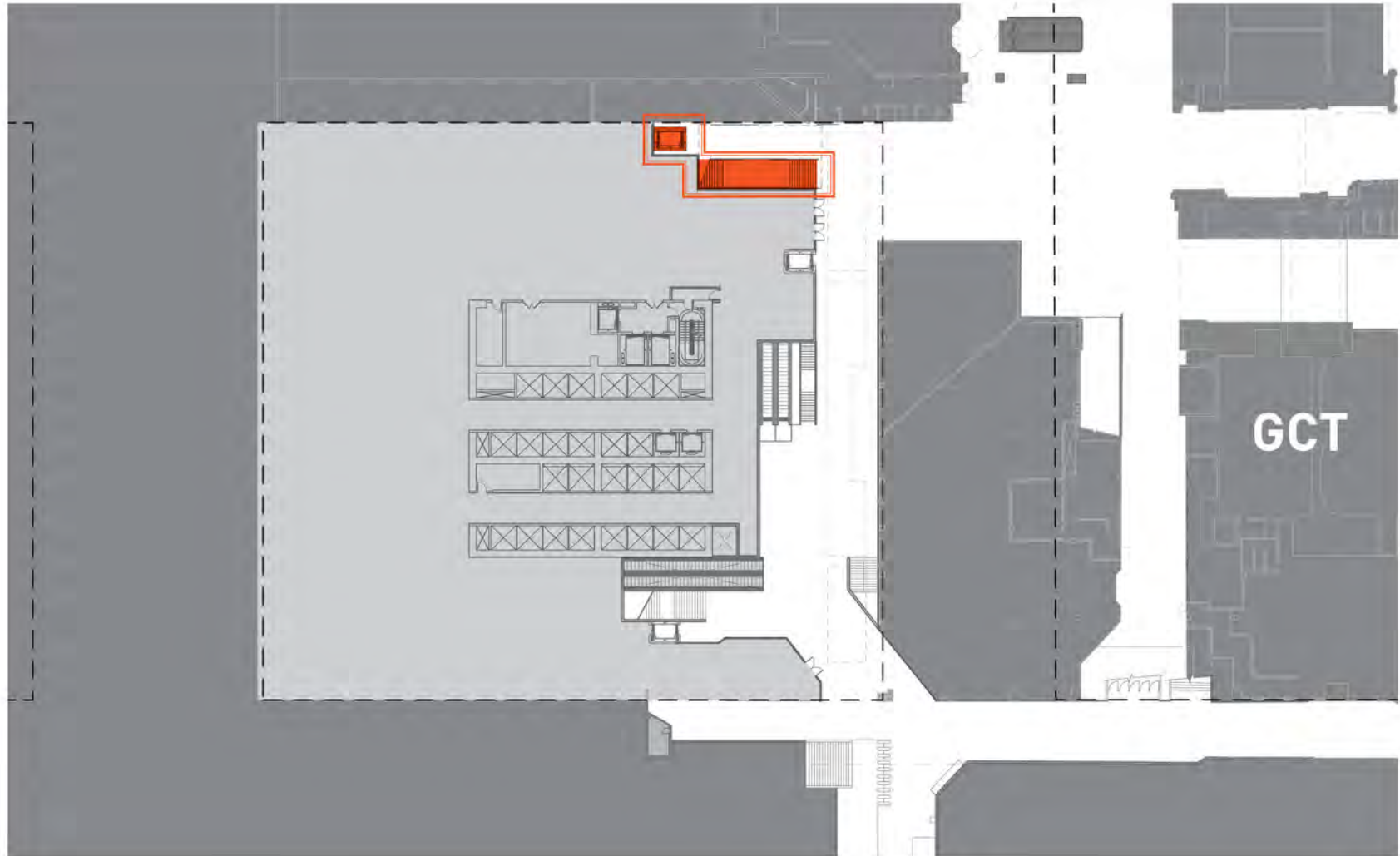
Floor Plan

- 1 GROUND FLOOR OF ONE VANDERBILT CONNECTION TO STREET

BONUSABLE IMPROVEMENTS

4

Onsite: Public Transit Hall



Key Map



Legend

Floor Plan

B1 BELOW GRADE OF ONE VANDERBILT
CONNECTION TO GCT MAIN CONCOURSE

BONUSABLE IMPROVEMENTS

Onsite: Public Transit Hall

4



TRANSIT HALL ELEVATION ON VANDERBILT

COST ESTIMATE

Onsite: Public Transit Hall

4

Category	Cost	Key Components of Work
Construction Cost	\$8,158,400	<ul style="list-style-type: none">– 4,000 square feet of pedestrian circulation space– Extra wide ornamental stair connecting GCT Concourse to Street– 35' high space at grade, with intricate ceiling design– 48' high space from GCT concourse– Feature wall 75' long x 35' high– Train schedule board and wayfinding– New elevator connecting Concourse to Street– New mechanical, electrical and sprinkler systems– New emergency communication systems– New lighting– New fire and smoke separation systems to isolate areas in the event of fire
Soft Cost (22%)	\$1,174,800	
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections		
Project Contingency (5%)	\$467,000	
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk		
Project Total Cost Estimate:	\$9,800.200	

VANDERBILT PLAZA

PUBLIC BENEFITS

Vanderbilt Avenue: Current Condition

5

- Congestion at corner of 42nd and Vanderbilt
- Overcrowding on Madison Avenue
- Bus stop on Madison Avenue

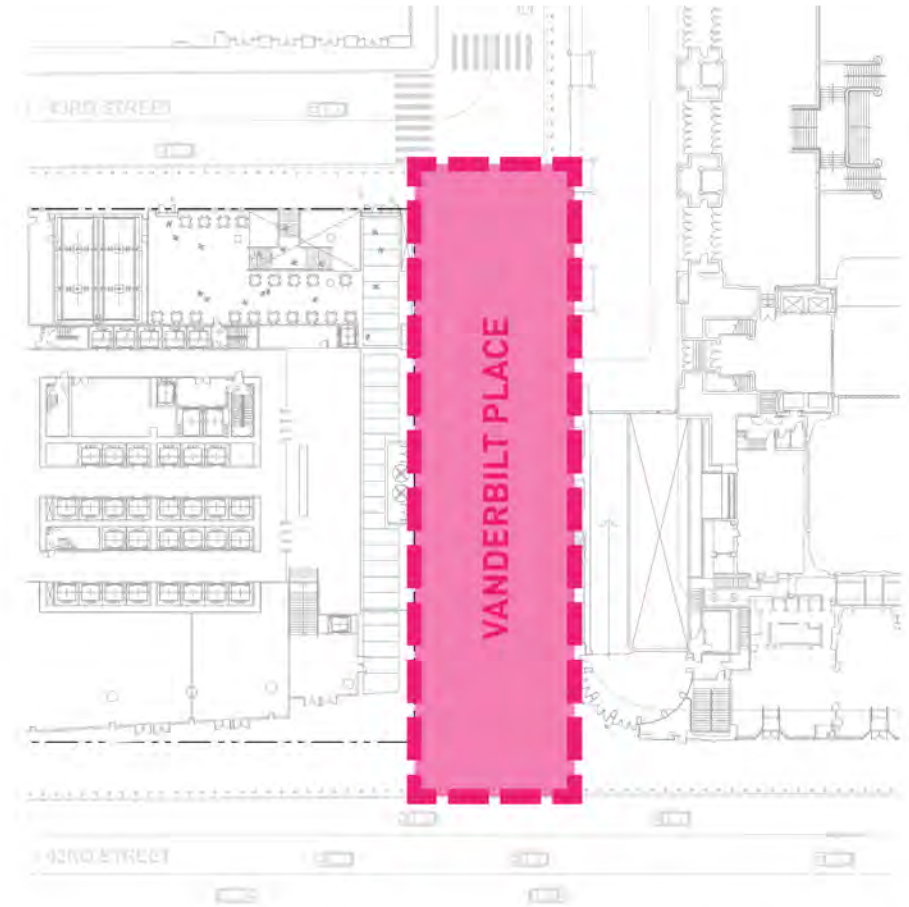


PUBLIC BENEFITS

Vanderbilt Avenue: Current Condition

5

- Highest long-term value as plaza integrated with GCT
- Functional pedestrian space
- New public foyer to Midtown & iconic GCT
- Eliminates pedestrian / vehicle conflicts
- Closure has a de minimis impact on local traffic flow



VANDERBILT PUBLIC PLACE

BONUSABLE IMPROVEMENT

Vanderbilt Public Place

5



AERIAL VIEW LOOKING NORTH FROM 42ND STREET

COST ESTIMATE

Offsite: Vanderbilt Pedestrian Plaza

5

Category	Cost
Construction Cost	\$8,979,400
<ul style="list-style-type: none">- Direct Trade Work- Contractors General Conditions- Contractor Bonds and Insurance- Contractor Overhead and Fee- Anticipated Costs to Finalize Design Details- Construction Phasing and Logistics- Temporary Protection- Safety	
Soft Cost (15%)	\$1,293,100
<ul style="list-style-type: none">- Design Fees- Project Management Fees- Permits- Legal- Builders Risk & Railroad Insurance- Review and approvals with DOT and PDC	
Project Contingency (5%)	\$513,600
<ul style="list-style-type: none">- Unforeseen Conditions- Overall Execution Risk	
Project Total Cost Estimate:	\$10,786,000

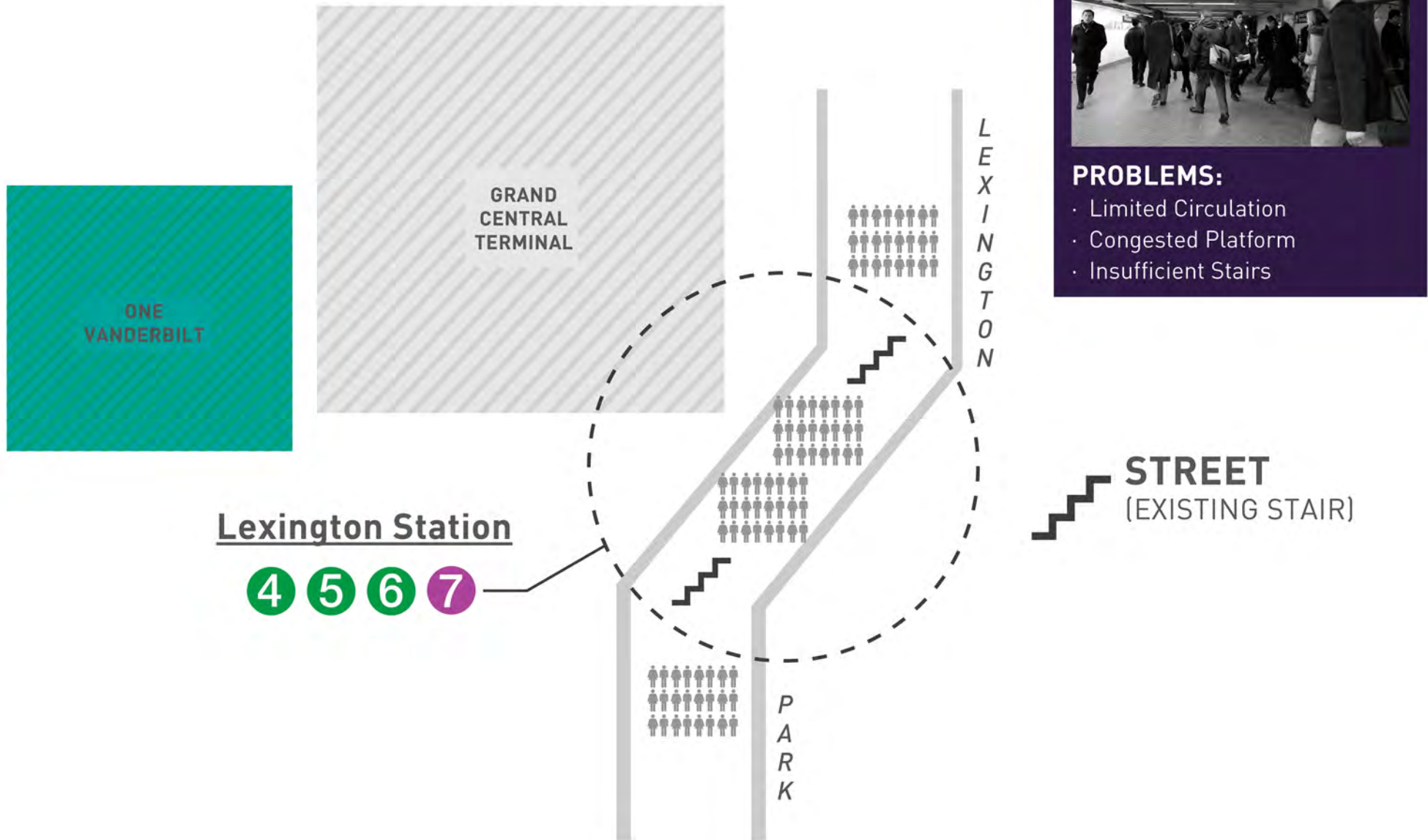
Key Components of Work

- 12,900 square feet of new custom paving with stone fascias and borders around main focal areas
- Water feature to be determined
- Granite curbing and retractable bollards at the perimeter to permit emergency access
- New catch basins and associated connections to existing storm drainage systems
- Custom seating - benches
- New lighting
- Cleaning and restoration of adjacent surfaces

4 / 5 / 6 / 7 Lexington Subway Station Improvements

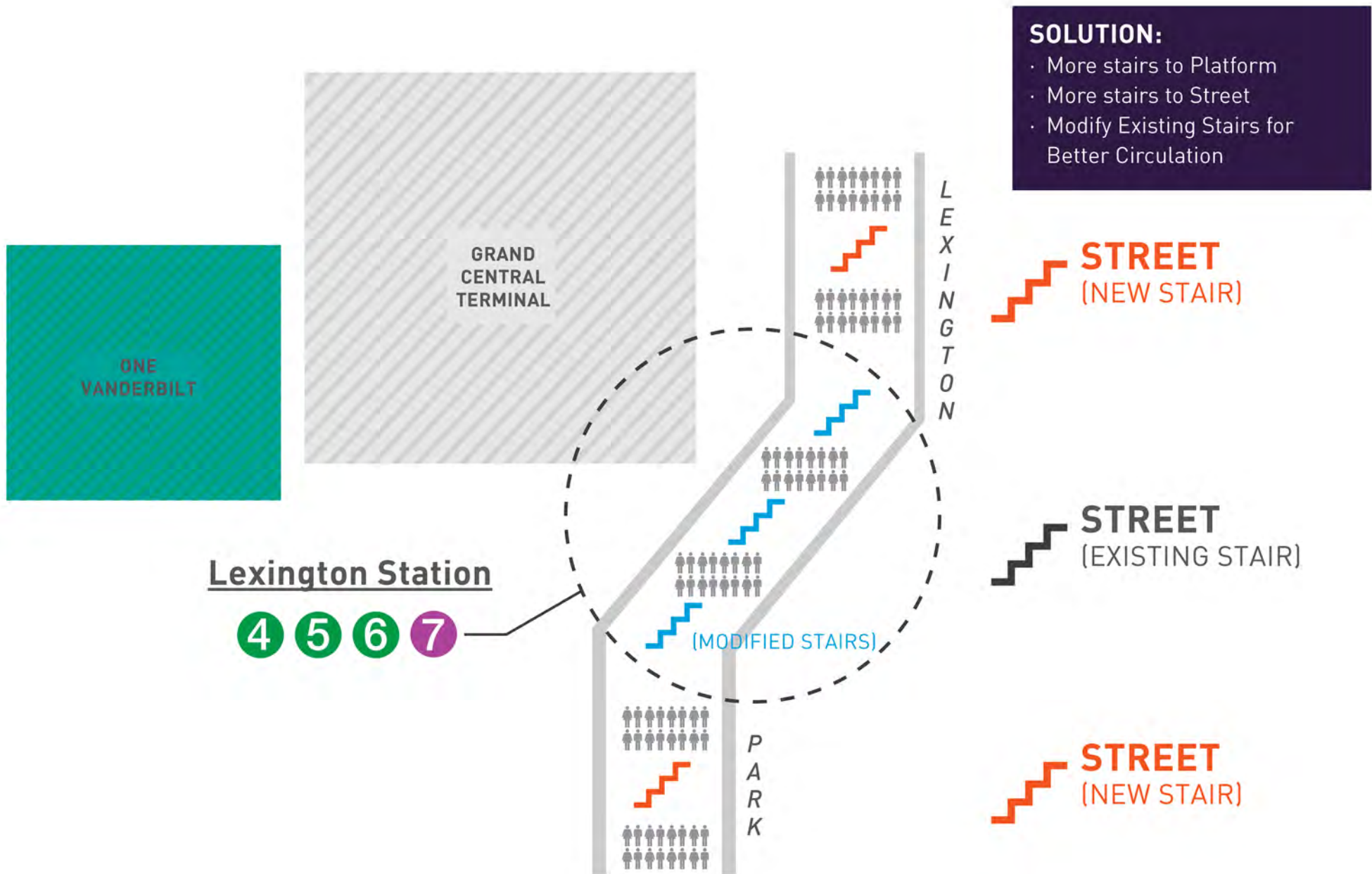
PUBLIC BENEFITS

4 / 5 / 6 / 7 at Grand Central Terminal: Current Condition



PUBLIC BENEFITS

4 / 5 / 6 / 7 at Grand Central Terminal: Solution

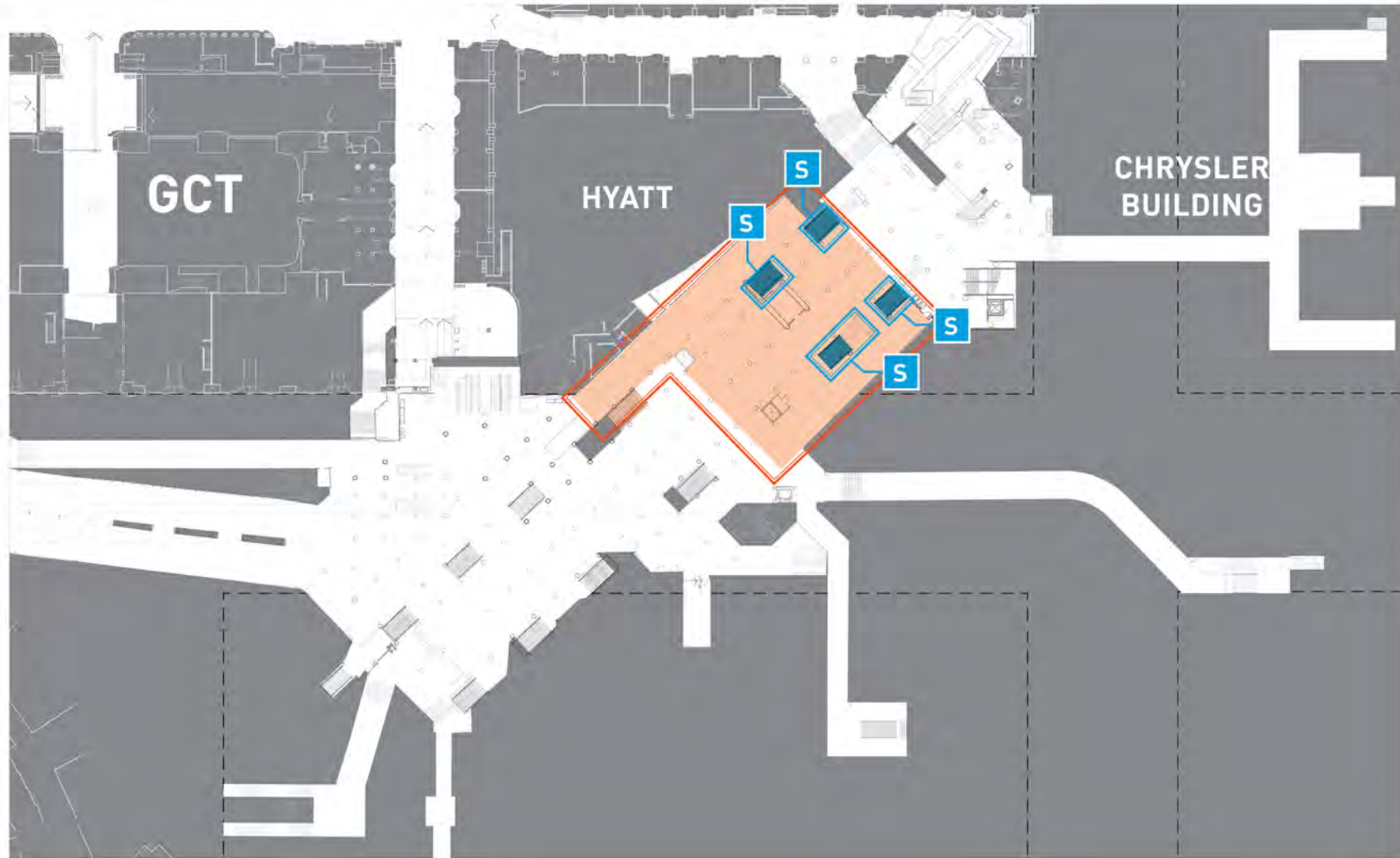


MEZZANANINE LEVEL CIRCULATION

BONUSABLE IMPROVEMENT

Offsite: Mezzanine Level Circulation

6



Key Map



Legend

- S** NEW STAIR
- E** NEW ENTRY
- C** NEW CONNECTION
- NEW CIRCULATION SPACE**
- S** IMPROVED STAIR
- E** IMPROVED ENTRY

Floor Plan

B1 / B2 CONCOURSE AT LEXINGTON STATION

BONUSABLE IMPROVEMENT

Offsite: Mezzanine Level Circulation

6



NORTH VIEW



COST ESTIMATE

Offsite: Mezzanine Level Circulation

6

Category	Cost
Construction Cost	\$30,916,250
<ul style="list-style-type: none">- Direct Trade Work- Contractors General Conditions- Contractor Bonds and Insurance- Contractor Overhead and Fee- Anticipated Costs to Finalize Design Details- Construction Phasing and Logistics- Temporary Protection- Safety	
Soft Cost (45%)	\$14,036,000
<ul style="list-style-type: none">- Design Fees- Project Management Fees- Permits- Legal- Builders Risk & Railroad Insurance- MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections• Track Flagmen	
Project Contingency (5%)	\$2,248,000
<ul style="list-style-type: none">- Unforeseen Conditions- Overall Execution Risk	
Project Total Cost Estimate:	\$47,200.250

Key Components of Work

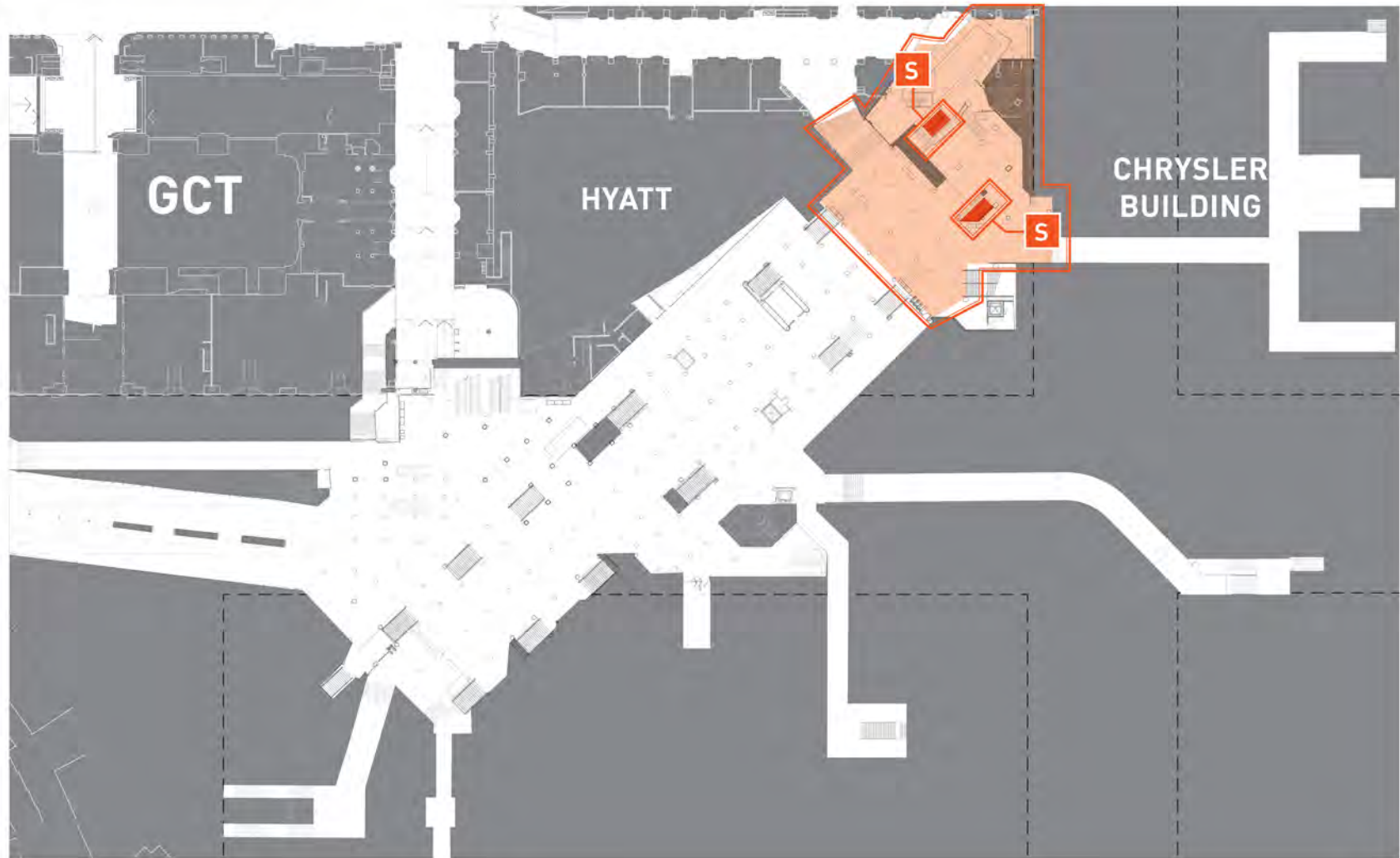
- 5,725 square feet of new finishes for newly created pedestrian circulation space
- 4,675 square feet of new finishes for renovation of existing circulation space
- (4) new relocated stairs connecting mezzanine level to platforms (P-20, P-21, P-22 & P-23)
- (4) new structurally framed openings for stairs and column thinning modifications for better circulation at the platform level
- Significant structural modifications of existing deep transfer girders from structure above for new circulation spaces
- Relocation of numerous existing mechanical, electrical and plumbing lines to create the space for new structural members to fit
- All structural and demolition work to be performed "by hand" due to limited space for equipment
- Multiple phases to minimize disruption
- Material deliveries to be done off-hours and likely by rail due to size and limited access from street

HYATT NORTH STAIRS

BONUSABLE IMPROVEMENT

Offsite: Hyatt North Stairs

7



Key Map



Legend

- | | | | |
|----------|-----------------------|----------|----------------|
| S | NEW STAIR | S | IMPROVED STAIR |
| E | NEW ENTRY | E | IMPROVED ENTRY |
| C | NEW CONNECTION | | |
| | NEW CIRCULATION SPACE | | |

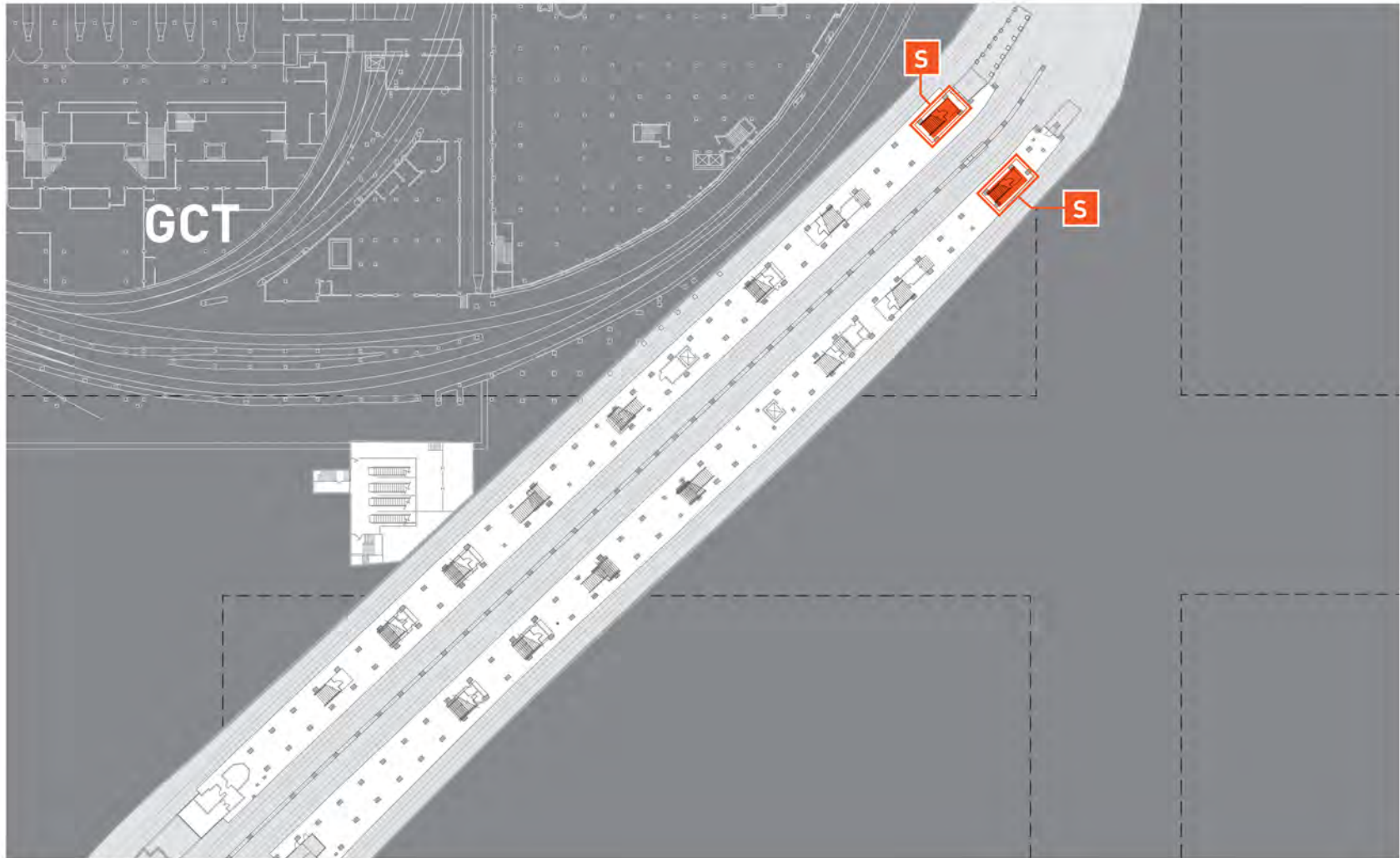
Floor Plan

B1 / B2 CONCOURSE AT LEXINGTON STATION

BONUSABLE IMPROVEMENT

Offsite: Hyatt North Stairs

7



Key Map



Legend

- S** NEW STAIR
- E** NEW ENTRY
- C** NEW CONNECTION
- NEW CIRCULATION SPACE**

- S** IMPROVED STAIR
- E** IMPROVED ENTRY

Floor Plan

B3 SUBWAY CONCOURSE AT LEXINGTON STATION

BONUSABLE IMPROVEMENT

Offsite: Hyatt North Stairs

7



SOUTH VIEW

COST ESTIMATE

Offsite: Hyatt North Stairs

7

Category

Cost

Key Components of Work

Construction Cost

\$24,487,000

- Direct Trade Work
- Contractors General Conditions
- Contractor Bonds and Insurance
- Contractor Overhead and Fee
- Anticipated Costs to Finalize Design Details
- Construction Phasing and Logistics
- Temporary Protection
- Safety

- 2,750 square feet of new finishes for newly created pedestrian circulation space serving new stairs
- 5,250 square feet of new finishes for renovation of existing circulation space (Control Area 240)
- (2) new stairs connecting mezzanine level to platforms (P-24 & P-25)
- (2) new structurally framed openings for stairs
- Significant structural modifications of existing deep transfer girders from structure above for new circulation spaces
- Relocation of numerous existing mechanical, electrical and plumbing lines to create the space for new structural members to fit
- All structural and demolition work to be performed “by hand” due to limited space for equipment
- Multiple phases to minimize disruption
- (8) new turnstiles, (2) gates and new agent booth
- Material deliveries to be done off-hours and likely by rail due to size and limited access from street

Soft Cost (45%)

\$11,117,000

- Design Fees
- Project Management Fees
- Permits
- Legal
- Builders Risk & Railroad Insurance
- MTA Force Account
 - Engineering Fees & Approvals
 - Inspections
 - Track Flagmen

Project Contingency (5%)

\$1,780,200

- Unforeseen Conditions
- Overall Execution Risk

Project Total Cost Estimate:

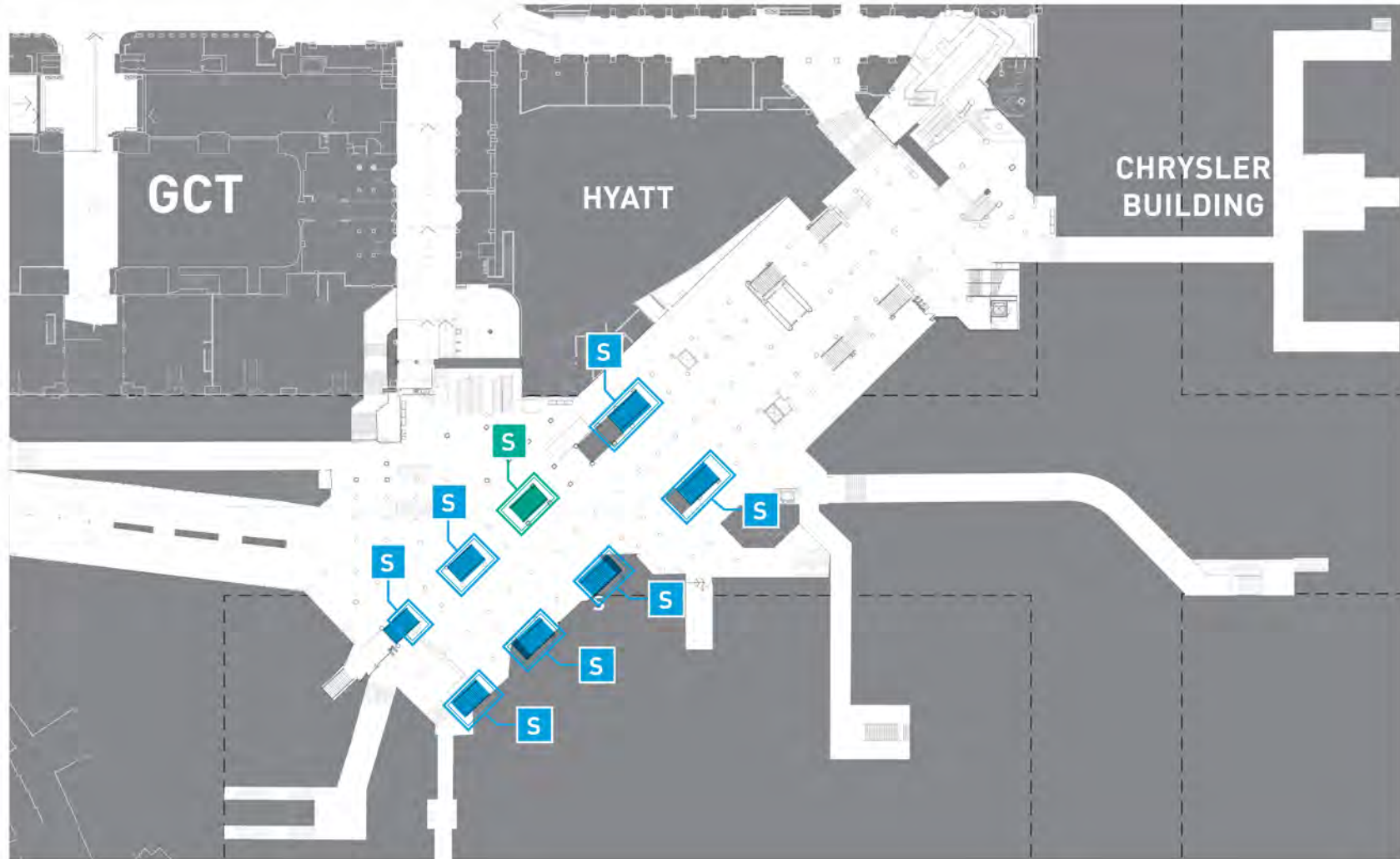
\$37,384,200

MODIFICATIONS of EXISTING STAIRS to PLATFORM

BONUSABLE IMPROVEMENT

Offsite: Modifications to Existing Stairs to Platform

8



Key Map



Legend

- | | |
|-------------------------|-------------------------------|
| S NEW STAIR | S IMPROVED STAIR |
| E NEW ENTRY | E IMPROVED ENTRY |
| C NEW CONNECTION | S IMPROVED STAIR (MTA) |
| NEW CIRCULATION SPACE | |

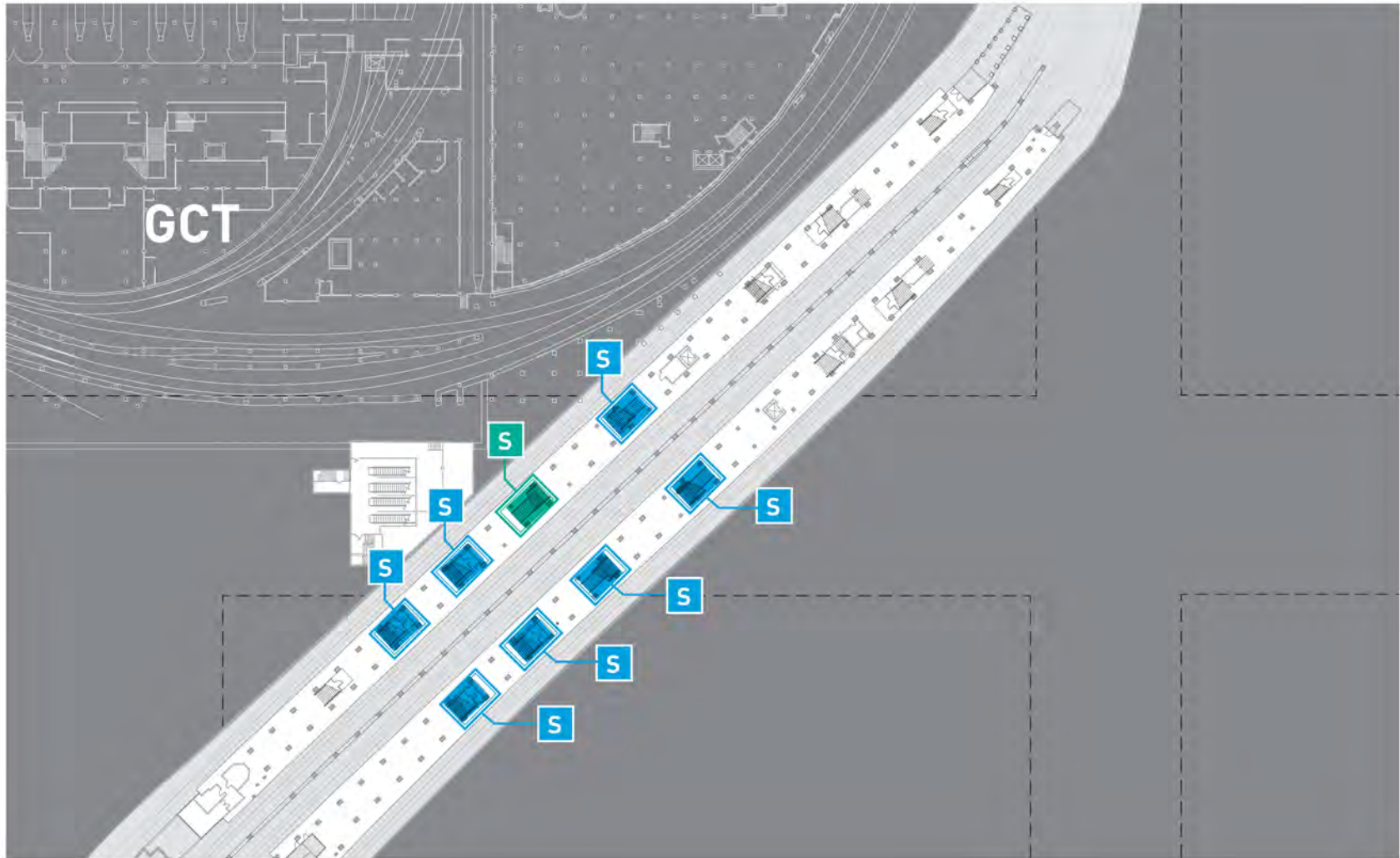
Floor Plan

B1 / B2 CONCOURSE AT LEXINGTON STATION

BONUSABLE IMPROVEMENT

Offsite: Modifications to Existing Stairs to Platform

8



Key Map



Legend

- | | |
|---|-------------------------------|
| S NEW STAIR | S IMPROVED STAIR |
| E NEW ENTRY | E IMPROVED ENTRY |
| C NEW CONNECTION | S IMPROVED STAIR (MTA) |
|  NEW CIRCULATION SPACE | |

Floor Plan

B3 SUBWAY CONCOURSE AT LEXINGTON STATION

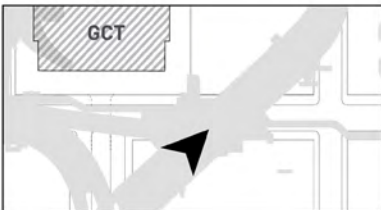
BONUSABLE IMPROVEMENT

Offsite: Modifications to Existing Stairs to Platform

8



NORTH EAST VIEW



COST ESTIMATE

8

Offsite: Modifications of Existing Stairs to Platform

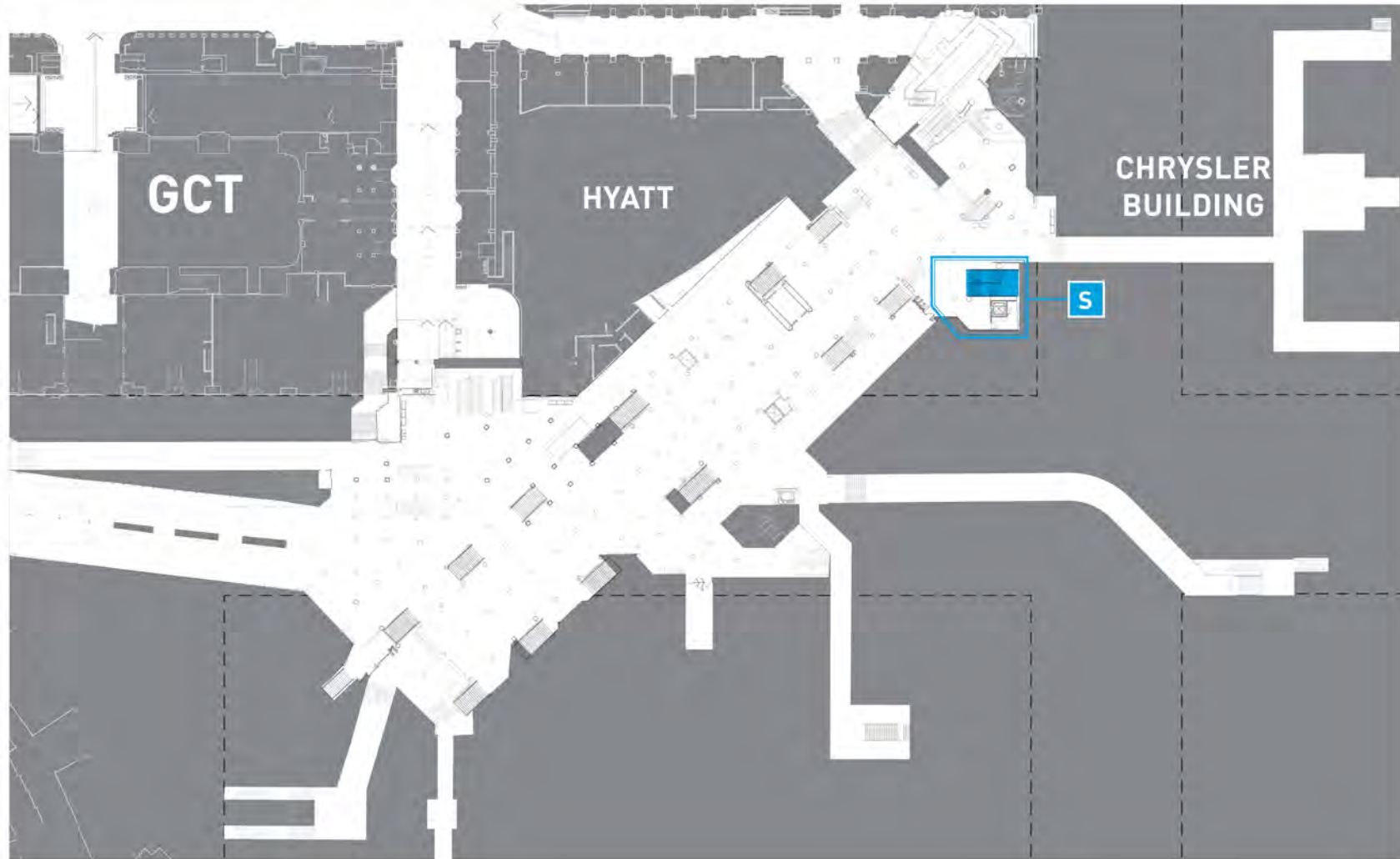
Category	Cost	Key Components of Work
Construction Cost	\$17,970,000	<ul style="list-style-type: none">– Removal and installation of (7) new stairs (P-12, P-13, P-14, P-15, P-17, P-18, P-19)– Surgical structural modifications of existing stair columns (25 total) involving installation of steel reinforcements to column flanges and webs to allow the removal of corresponding steel resulting in the reduction of each columns overall size several inches– Construction of new grillage beams below the platform at the base of each column to accept new column configuration– Application of intumescent fireproofing– Relocation of existing mechanical, electrical and plumbing lines to create the space for new structural members to fit– All structural and demolition work to be performed “by hand” due to limited space for equipment– Multiple phases to minimize disruption– Material deliveries to be done off-hours and likely by rail due to size and limited access from street
Soft Cost (45%)	\$8,158,500	
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections• Track Flagmen		
Project Contingency (5%)	\$1,306,000	
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk		
Project Total Cost Estimate:	\$27,434,500	

REPLACEMENT of STREET ENTRANCE at LEXINGTON (Strawberry Stair)

BONUSABLE IMPROVEMENT

Offsite: Replacement of Street Entrance at Lexington

9



Key Map



Legend

- | | | | |
|----------|-----------------------|----------|----------------|
| S | NEW STAIR | S | IMPROVED STAIR |
| E | NEW ENTRY | E | IMPROVED ENTRY |
| C | NEW CONNECTION | | |
| | NEW CIRCULATION SPACE | | |

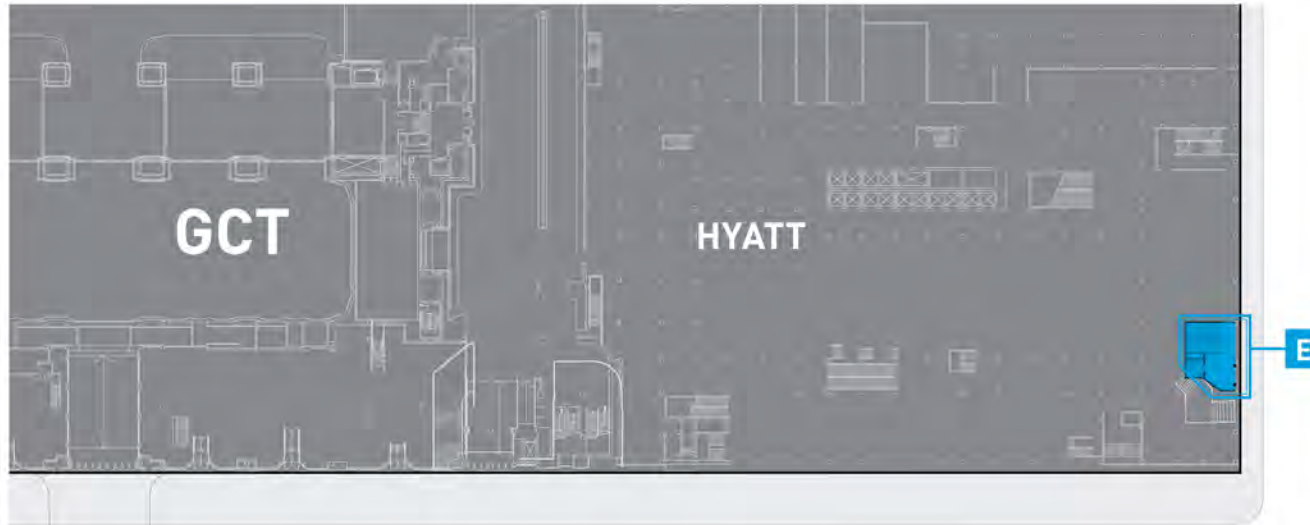
Floor Plan

B1 / B2 CONCOURSE AT LEXINGTON STATION

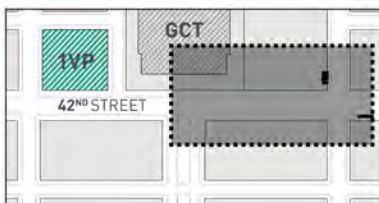
BONUSABLE IMPROVEMENT

Offsite: Replacement of Street Entrance at Lexington

9



Key Map



Legend

- S** NEW STAIR
- E** NEW ENTRY
- C** NEW CONNECTION
- NEW CIRCULATION SPACE**
- S** IMPROVED STAIR
- E** IMPROVED ENTRY

Floor Plan

- 1** AT GRADE ENTRANCE AT LEXINGTON STATION

COST ESTIMATE

9

Offsite: Replacement of Street Entrance at Lexington (Strawberry)

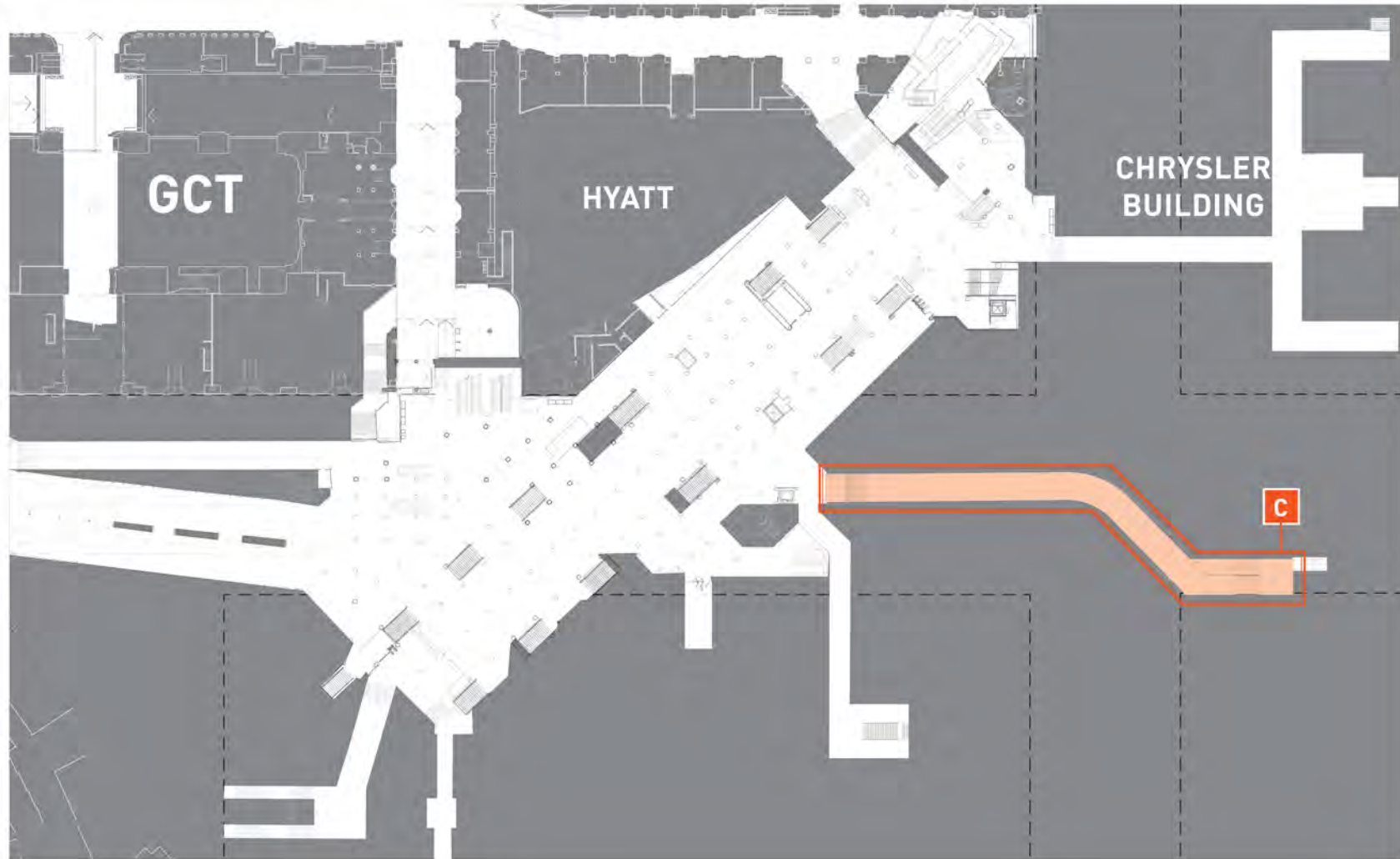
Category	Cost	Key Components of Work
Construction Cost	\$9,513,000	<ul style="list-style-type: none">– Installation of new elevator pit foundation to be done in tight conditions requiring hand excavation– Removal of existing stair and installation of new stair 12'-13' in width connecting Lexington to mezzanine level– Installation of (12) tons of new structural steel framing to support new stair and landing configuration
Soft Cost (45%)	\$4,319,000	<ul style="list-style-type: none">– New transit grade 2-stop glass cab with glass elevator enclosure– New building storefronts on Lexington to accommodate new configuration– Fitout of finished areas at each elevator landing– Relocation of existing utilities and existing mechanical, electrical and plumbing lines to create the space for new structural members to fit
Project Contingency (5%)	\$691,500	<ul style="list-style-type: none">– Relocate (11) turnstiles and install (8) new turnstiles at the new landing on the mezzanine level serving area 240
Project Total Cost Estimate:	\$14,523,500	

NEW STREET STAIRS and PASSAGEWAY at MOBIL

BONUSABLE IMPROVEMENT

Offsite: New Street Stairs and Passageway at Mobil

10



Key Map



Legend

- | | | | |
|----------|-----------------------|----------|----------------|
| S | NEW STAIR | S | IMPROVED STAIR |
| E | NEW ENTRY | E | IMPROVED ENTRY |
| C | NEW CONNECTION | | |
| | NEW CIRCULATION SPACE | | |

Floor Plan

B1 / B2 CONCOURSE AT LEXINGTON STATION

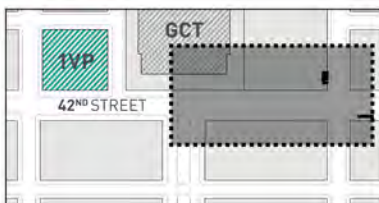
BONUSABLE IMPROVEMENT

Offsite: New Street Stairs and Passageway at Mobil

10



Key Map



Legend

- S** NEW STAIR
- E** NEW ENTRY
- C** NEW CONNECTION
- NEW CIRCULATION SPACE**
- S** IMPROVED STAIR
- E** IMPROVED ENTRY

Floor Plan

- 1** AT GRADE ENTRANCE AT LEXINGTON STATION

BONUSABLE IMPROVEMENT

Offsite: New Street Stairs and Passageway at Mobil

10



SOUTH WEST VIEW

COST ESTIMATE

Offsite: New Street Stair and Passageway at Mobil

10

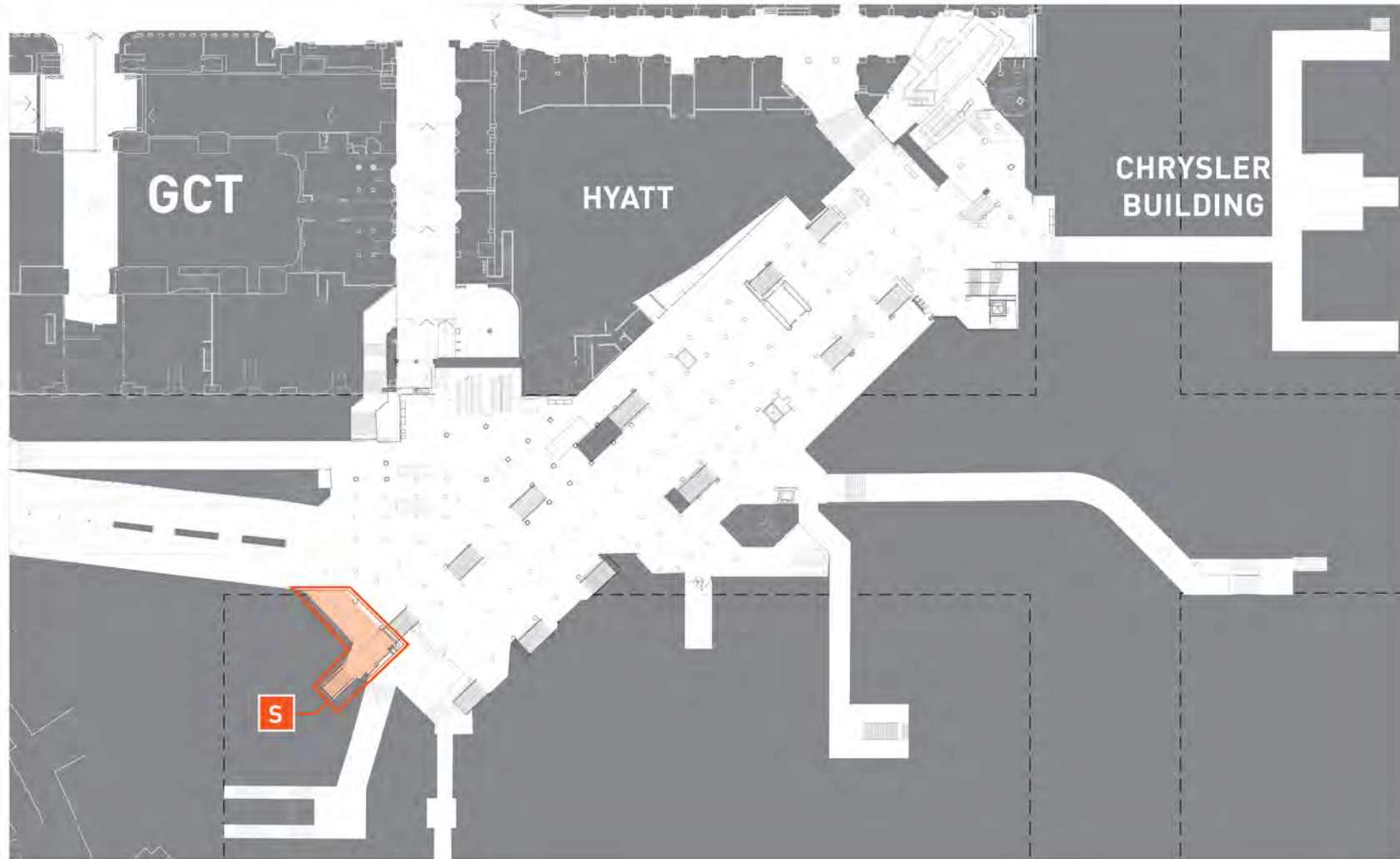
Category	Cost	Key Components of Work
Construction Cost	\$4,812,600	<ul style="list-style-type: none">– (2) new 5'-0" wide entrance stairs from street– Installation of (13) tons new structural steel to frame openings below sidewalk level to accept stair construction– Clean, cutout and patch floor, wall and ceiling finishes in existing Mobil passage of 4,100 square feet– Relocate existing street utilities including fire hydrant to allow installation of new stairs– Reconfigure existing turnstiles and fare control equipment in mezzanine level for new entrance configuration
Soft Cost (45%)	\$2,185,000	
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections• Track Flagmen		
Project Contingency (5%)	\$349,900	
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk		
Project Total Cost Estimate:	\$7,347,500	

NEW STAIR TO MEZZANINE at 125 PARK

BONUSABLE IMPROVEMENT

Offsite: New Stair to Mezzanine at 125 Park

11



Key Map



Legend

- S** NEW STAIR
- E** NEW ENTRY
- C** NEW CONNECTION
- NEW CIRCULATION SPACE**
- S** IMPROVED STAIR
- E** IMPROVED ENTRY

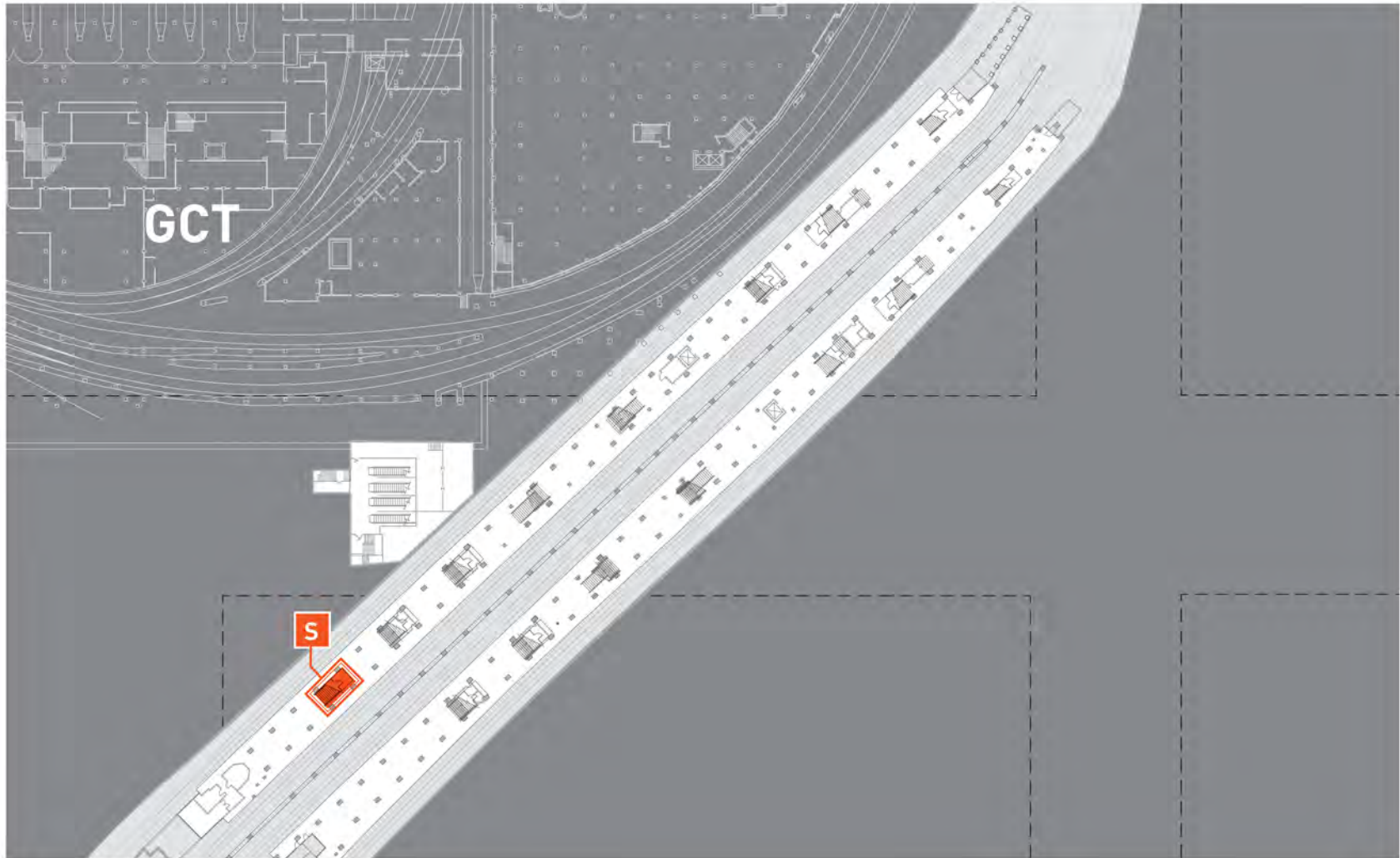
Floor Plan

B1 / B2 CONCOURSE AT LEXINGTON STATION

BONUSABLE IMPROVEMENT

Offsite: New Stair to Mezzanine at 125 Park

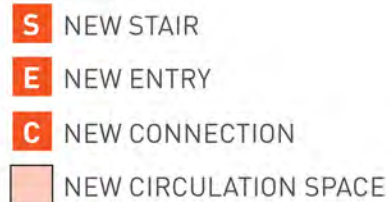
11



Key Map



Legend



Floor Plan

B3 SUBWAY CONCOURSE AT LEXINGTON STATION

COST ESTIMATE

Offsite: New Stair to Platform at 125 Park

11

Category	Cost
Construction Cost	\$3,479,000
<ul style="list-style-type: none">– Direct Trade Work– Contractors General Conditions– Contractor Bonds and Insurance– Contractor Overhead and Fee– Anticipated Costs to Finalize Design Details– Construction Phasing and Logistics– Temporary Protection– Safety	
Soft Cost (45%)	\$1,579,500
<ul style="list-style-type: none">– Design Fees– Project Management Fees– Permits– Legal– Builders Risk & Railroad Insurance– MTA Force Account<ul style="list-style-type: none">• Engineering Fees & Approvals• Inspections• Track Flagmen	
Project Contingency (5%)	\$253,000
<ul style="list-style-type: none">– Unforeseen Conditions– Overall Execution Risk	
Project Total Cost Estimate:	\$5,311,500

Key Components of Work

- Excavation for new concrete foundation including footings, walls and overhead slab
- New 8'-0" wide entrance stairs between mezzanine and platform
- New finishes for 926 square feet of new mezzanine level space
- Clean, cutout and patch floor, wall and ceiling finishes to meet existing finishes
- Relocate existing mechanical, electrical and plumbing services to install new steel frame opening to accept new stair

ONE VANDERBILT

